

Draft Final

Local Character Areas

14 December 2020

For the City of Canterbury Bankstown

Prepared by

OLSSON

ARCHITECTURE | URBAN PROJECTS

Title	Report
Project	Special Character Areas
Description	Draft Final Report
Key Contacts	Lisa Ho & Mauricio Tapia
	T 02 9707 9923
Prepared By	OLSSON Architecture & Urban Projects
Address	Level 4 / 68-72 Wentworth Avenue Surry Hills NSW 2010
Email	russell@olssonassociates.com.au
Website	olssonassociates.com.au
Author	RO/TC
Checked	RO
Authorised	RO

Revision History	A	B
Issued To Date	01/09/20	14/12/20
Document Author	RO/LT	RO/TC
Document checked	RO	RO

CONTENTS

1.0 INTRODUCTION AND PLANNING FRAMEWORK	04
1.1 Project background	04
1.2 Relationship to the Planning Framework	04
1.3 Relationship to the Connective City 2036 LSPS and the Housing Strategy	04
2.0 COMMUNITY CONSULTATION	05
3.0 EXTENT OF CHANGE IN THE LOCAL CHARACTER AREAS	06
3.1 DPIE Extent of Change Classification	06
3.2 Change	06
3.3 Enhance	06
3.4 Maintain	06
3.5 Extent of Change in the Local Character Areas	07
4.0 LOCAL CHARACTER AREAS	08
4.1 Local Character Area Overview	09
4.2 East Local Character Areas	10
4.3 West Local Character Areas	11
5.0 CHARACTER STATEMENTS	12
5.1 Croydon Park	13
5.2 Campsie and Canterbury East (Deferred)	19
5.3 Hurlstone Park and Canterbury North (Part Deferred)	26
5.4 Canterbury and North Earlwood	33
5.5 East Earlwood	40
5.6 South Earlwood	47
5.7 East Hills and South-West Panania	53
5.8 East Hills South and South Picnic Point	60
5.9 South-East Panania and West Picnic Point	67
5.10 East Picnic Point	74
5.11 Revesby South	81
5.12 Revesby Heights	87
5.13 Padstow	94
5.14 West Padstow Heights	101
5.15 Padstow Heights and East Padstow.	107

1.0 INTRODUCTION AND PLANNING FRAMEWORK

1.1 PROJECT BACKGROUND

The City of Canterbury Bankstown's Local Strategic Planning Statement (LSPS) Connective City 2036, establishes a single strategic planning vision for the Canterbury Bankstown Local Government Area. The strategy sets out the need to protect and enhance a number of attractive, low-density suburban areas within the Council area. The LSPS identified areas in the East and West of the LGA for their special natural, scenic or architectural value.

The eastern areas include parts of Croydon Park, Ashbury, Hurlstone Park, Campsie, Canterbury and Earlwood. The Western areas include parts of Padstow, Panania, East Hills, and Revesby. These areas feature unique characteristics and identifiable Local Character which should be safeguarded. Protecting these areas from inappropriate future development is an important component of the City of Canterbury Bankstown Housing Strategy.

The Local Character Statements have been prepared by building on the guidelines put forward by the NSW Department of Planning Industry and Environment within a framework that succinctly defines the Existing Character of each area. The framework sets a clear vision for the future by establishing Desired Future Character statements for each area. This study develops a series of evidence based recommended planning controls to ensure the areas will be protected and that future development will achieve the desired future character.

1.2 RELATIONSHIP TO THE PLANNING FRAMEWORK

In September 2018 Canterbury Bankstown Council Endorsed the project plan to undertake the Accelerated Local Environmental Plan Program to bring together the planning controls of the former Canterbury and Bankstown Councils into a single consolidated Local Environmental Plan and Development Control Plan. Council prepared its Local Strategic Planning Statement, the Connective City 2036, to form the evidence base and strategic justification for subsequent changes to Councils Local Environment Plan (2020 CBC LEP) and the consolidated Development Control Plan (DCP). The Draft Consolidated LEP is currently with the Department of Planning for finalisation. The recommended planning controls within this study are to form part of the planning proposal to amend the 2020 CBC LEP where they will be included in the LEP written instrument and maps, and form part of the DCP.

1.3 RELATIONSHIP TO THE CONNECTIVE CITY 2036 LSPS AND THE HOUSING STRATEGY

The LSPS Connective City 2036 must be considered as part of the LEP making process and forms part of the strategic merit test for a Gateway Determination Under S3.34 of the EP&A Act 1979.

The LSPS includes in its aim to *"Define and protect our distinctiveness, areas with a strong sense of character and unique features"* The Housing in the City Plan 2036 within the LSPS identifies indicative Local Character Areas.

The Canterbury Bankstown Housing Strategy was adopted by council on 23 June 2020. The Housing Strategy notes that *"Community consultation on past strategies or proposals indicates that the community is passionate about the areas they live in. Maintaining local character and recognising the importance of place must be a key determinant in the location of new housing growth"*

2.0 COMMUNITY CONSULTATION

2.0 COMMUNITY CONSULTATION

Community consultation is fundamental to understanding the Local Character of an Area. It helps to identify what elements are important to residents and it ensures that the future vision for each area has regard to the community's wants, needs and ambitions. Council engaged JOC Consulting to undertake the consultation, and The City of Canterbury Bankstown Special Character Areas workshop Outcomes Report, December 2019, supports the Local Character Area Study.

Residents from each area were selected and invited to two workshops, one for the eastern area and one for the western area, where they were invited to provide input and share ideas about the character areas through a series of activities and mapping exercises.

Residents expressed why local character is important, and which aspects they wanted to maintain, protect and enhance of each area. The importance of the natural environment and green space, amenity, density, built form and architecture were used to help define each Local Character Area. How these elements come together to contribute to the unique look and identity of a place, how they form a connection to the past and heritage, and how an area fosters a sense of community, were also less tangible but important consideration in defining each place.

When asked for their future vision for the SCA's participants put forward recurring themes for all the areas which included a desire to maintain the low-density residential character of the areas. These considerations included protecting the heritage of an area by discouraging incompatible development and poor design outcomes which would impact the amenity. Other aspects of character included ensuring that the 'openness' of the areas between the houses is retained, and that high quality streetscapes will allow for trees and nature. Future development should be environmentally sustainable and views within and from areas needed to be protected. The impact of vehicles and garages on the streetscapes of recently developed areas was also considered to reduce the amenity of certain areas. These themes along with area specific feedback were used to help shape and define the Desired Future Character Statements for each SCA.



Community Consultation for the Eastern Local Character areas held at the Magpie Sports Club November 2019



Community Consultation for the Western Local Character areas held at the Revesby Workers Club November 2019

3.0 EXTENT OF CHANGE IN THE LCA'S

3.1 DPIE EXTENT OF CHANGE CLASSIFICATION

The NSW Department of Planning, Industry, and Environments (DPIE) Local Character Area and Place Guidelines contains three characterisations which provides a high level indication of the extent of change that an area can accommodate. These are:

- Change;
- Enhance; and
- Maintain.

3.2 CHANGE

The Local Character Areas cover a relatively small area within the Canterbury Bankstown local government area. The Local Character Areas only apply to residential land zones. Within the areas are a number of parks and reserves, corner shops, clubs and the like, however the areas are almost exclusively low density housing. There are no large industrial sites that may be considered for rezoning in the future.

The DPIE category of Change accommodates relatively substantial change which may possibly require the rezoning of large areas of non-residential land. No sites such as those have been identified in the course of this study, and no recommendations have been made to implement the Change category.

3.3 ENHANCE

The DPIE category of Enhance involves intensifying, improving or increasing the quality and experience of an area and should be compatible with existing and future land uses, predominant built forms and scale. The Enhance category has been applied to those Local Character Areas where the existing housing types, built form, street address and public realm have a heterogeneous character. These diverse outcomes may be due to a steep topography or a history of different building types developing over time. The streetscape and public and private domain interface outcomes are mixed and could be enhanced to create a more unified character in the future.

The western Local Character Areas in the vicinity of the Georges River contain a public realm and street addresses that have been undergoing change in recent years due to a substantial increase in dual occupancy development. The amenity of these streets has been reduced and the design quality dual occupancy development should be enhanced in future development.

3.4 MAINTAIN

The large majority of Local Character Areas have a character that it would be desirable to maintain. The eastern areas adjacent to the Cooks River and parts of the western areas have a public realm, street addresses, and built forms that are cohesive. Future development in these Local Character Areas is to generally maintain the existing character.

3.0 EXTENT OF CHANGE IN THE LCA'S

3.5 EXTENT OF CHANGE IN THE LOCAL CHARACTER AREAS

	Local Character Area	Character Principle
01	Croydon Park	Maintain
02	Campsie and Canterbury East (Deferred)	Maintain
03	Hurlstone Park and Canterbury North (Part Deferred)	Maintain
04	Canterbury and North Earlwood	Maintain
05	East Earlwood	Maintain
06	South Earlwood	Maintain
07	East Hills and South-West Panania	Enhance
08	East Hills South and South Picnic Point	Enhance
09	South East Panania and West Picnic Point	Enhance
10	East Picnic Point	Enhance
11	Revesby South	Enhance
12	Revesby Heights	Enhance
13	Padstow	Enhance
14	West Padstow Heights	Maintain
15	Padstow Heights and East Padstow	Maintain

4.0 LOCAL CHARACTER AREAS

4.0 LOCAL CHARACTER AREAS

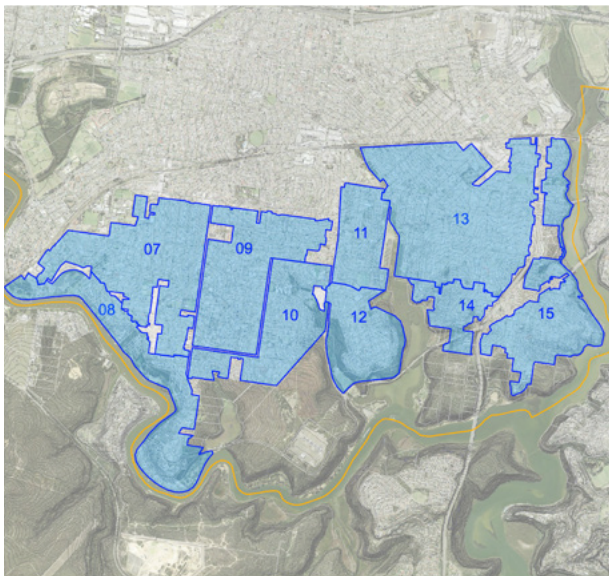
4.1 LOCAL CHARACTER AREAS OVERVIEW

The Local Character Areas are multi-layered and diverse environments that feature a range of elements from both the built and natural environment which have combined to create unified and distinctive areas of high residential amenity within the Canterbury Bankstown LGA.

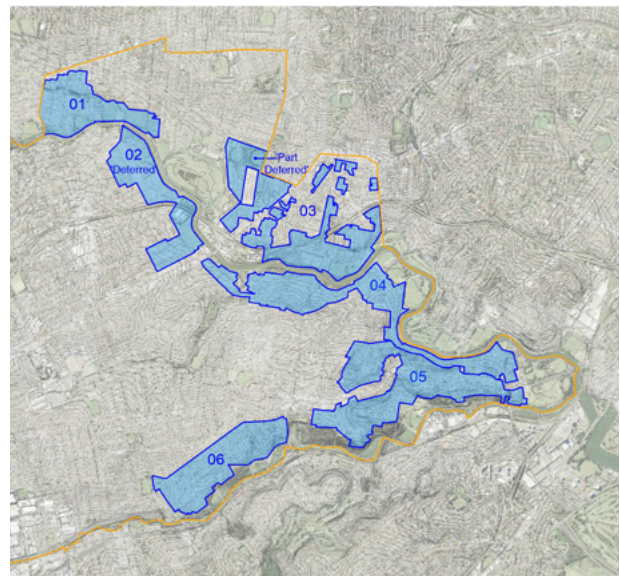
The LCAs are bound by a unique natural setting of rivers, parks and bushland. The visually distinctive topography characterises within the areas influence many aspects of the urban environment from the evolution of the road networks and the shape of urban block forms, to the height and the siting of houses and their relationship with the public domain. The low density residential housing features a landscaped setting and consistent scale, built form and materiality within each of the character areas, with some benefiting from the presence of individual contributory items. The extensive network of parks and reserves are linked by a network of high-quality streets which encourage active lifestyles and support passive outdoors activities within a green and secure setting. The streets feature almost continuous green edges as the extensive soft landscape and gardens within the private domain frame the open look and feel of the pedestrian environment.

It is important that the individual components that make up the LCAs are not eroded by future development as each forms part of a set of interrelated layers that form the distinctive character of the LCAs. The natural setting of LCAs, and the distinct topography are unique elements that identify and define the setting of each area and need to be protected.

The scale and character and design quality of the built form should remain consistent or enhanced as needed, to ensure that streetscapes remain at a high quality. The residential density should remain low to ensure the residential amenity is not adversely effected by an increase of incompatible development types. The balance between the built form and the landscaped setting is key to the strong sense of place within these high-quality environments and should be maintained and strengthened. The quality of the landscaped streets, the public open spaces, and the extensive tree canopies that surround them area fundamental to the sustainability of the LCAs.

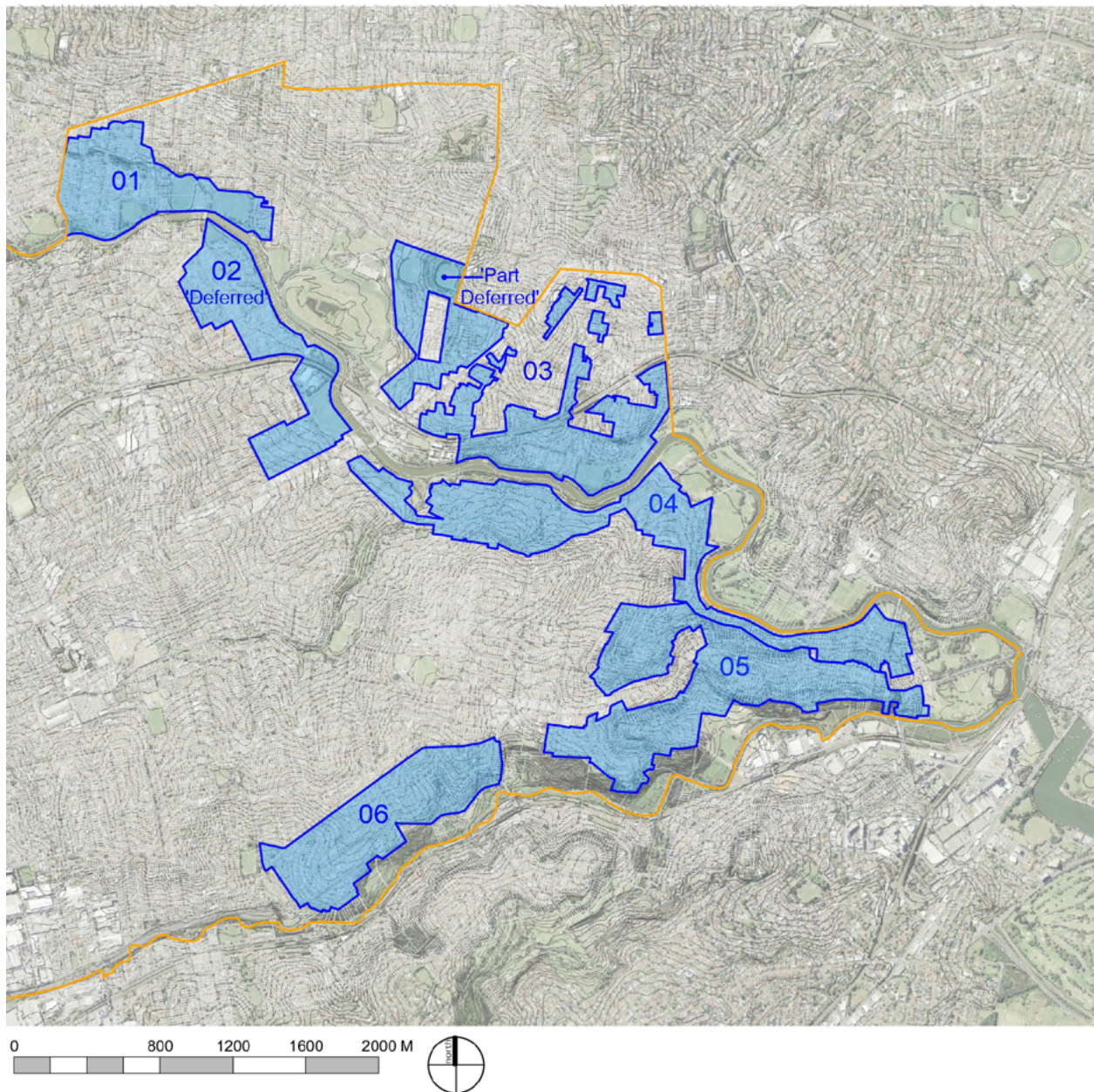


Western Local Character Areas



Eastern Local Character Areas

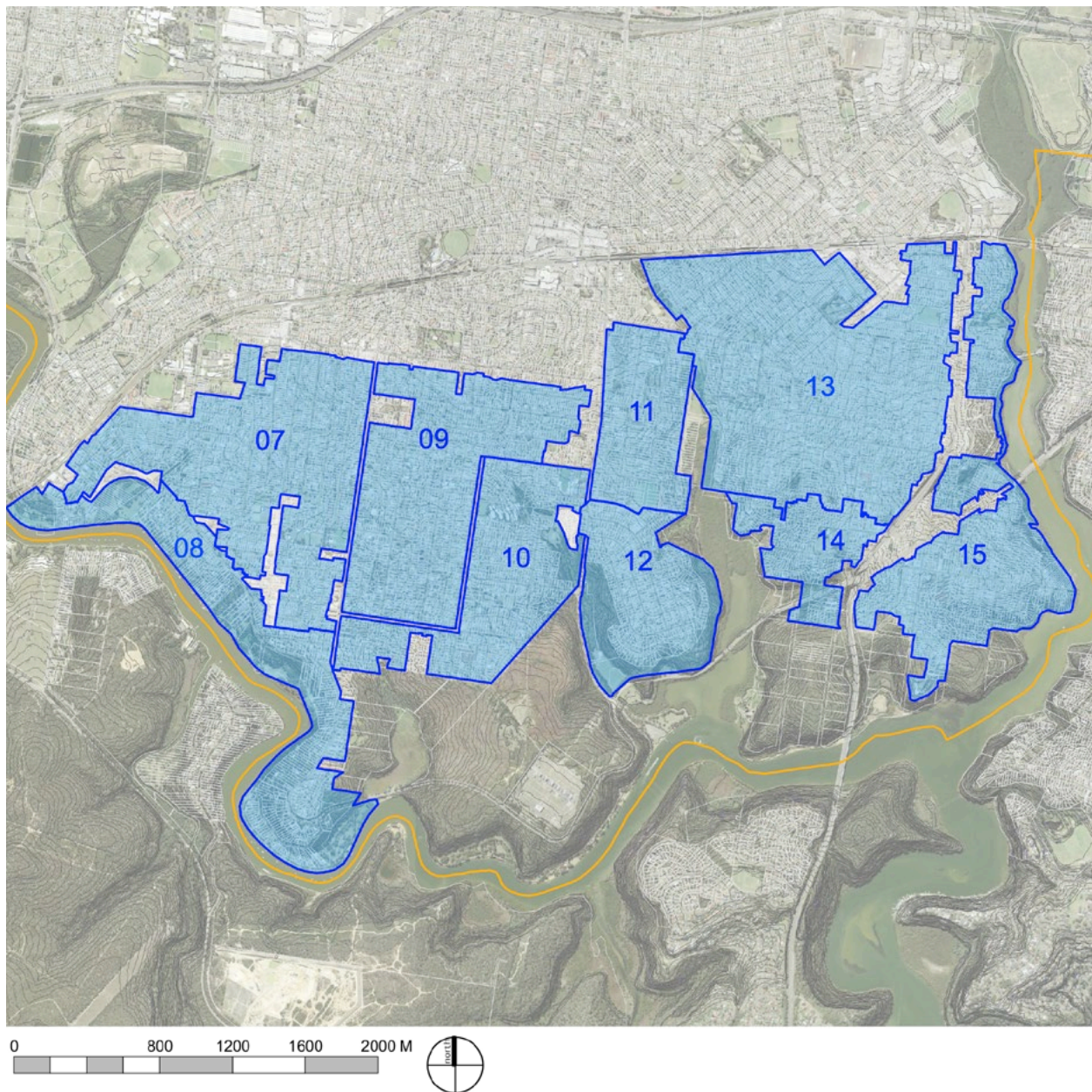
4.0 LOCAL CHARACTER AREAS



4.2 EAST LOCAL CHARACTER AREAS

- 01. Croydon Park
- 02. Campsie and Canterbury East (Deferred)
- 03. Hurlstone Park and Canterbury North (Part Deferred)
- 04. Canterbury and North Earlwood
- 05. East Earlwood
- 06. South Earlwood

4.0 LOCAL CHARACTER AREAS

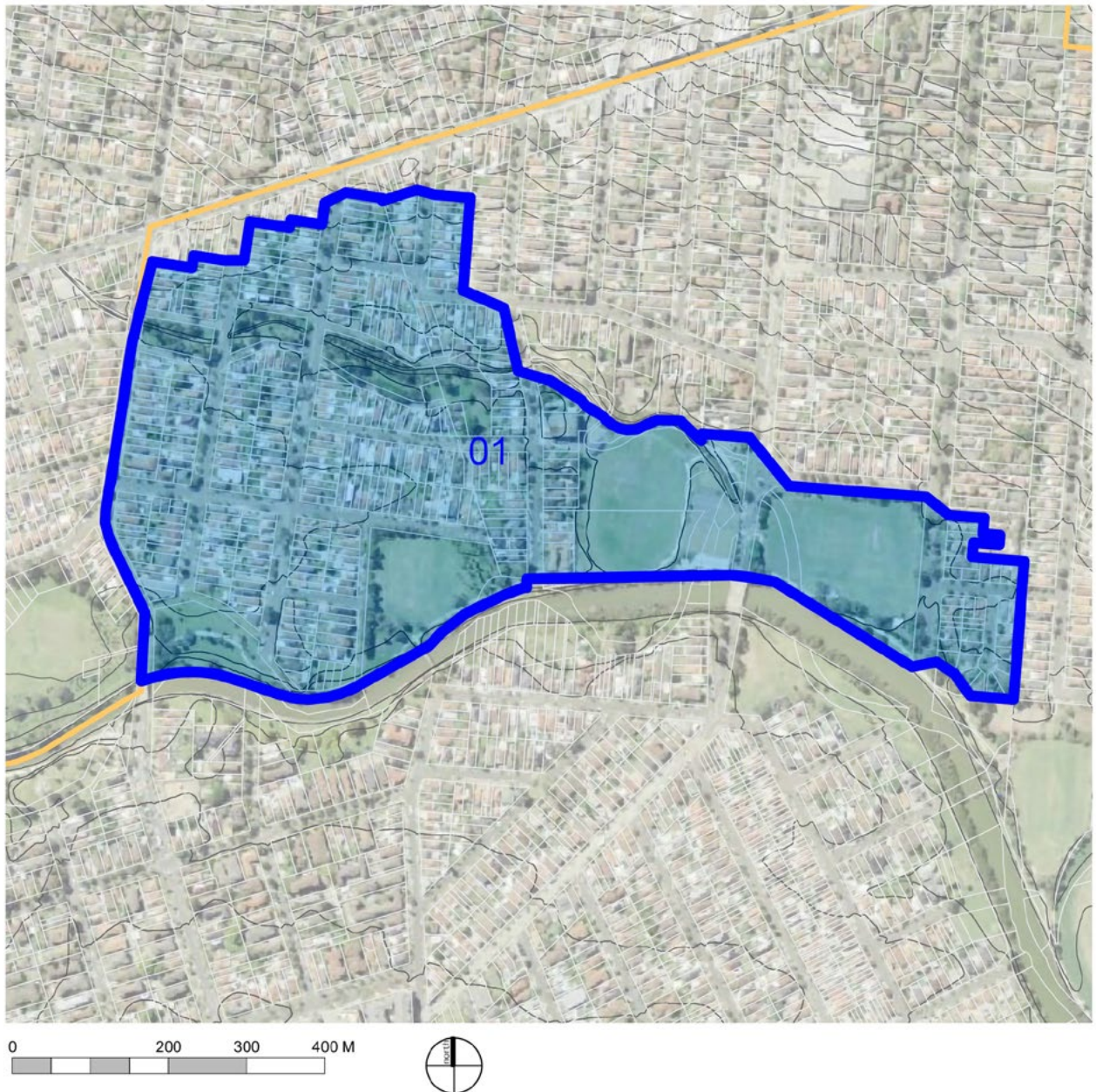


4.3 WEST LOCAL CHARACTER AREAS

- 07. East Hills and South West Panania
- 08. East Hills South and South Picnic Point
- 09. South East Panania and West Picnic Point
- 10. East Picnic Point
- 11. Revesby South
- 12. Revesby Heights
- 13. Padstow
- 14. West Padstow Heights
- 15. Padstow Heights and East Padstow.

5.0 LOCAL CHARACTER STATEMENTS

5.1 LOCAL CHARACTER STATEMENT



CROYDON PARK

The Croydon Park Local Character Area is located at the North East Boundary of Canterbury Bankstown LGA. The site is bound to the east by the Ashbury Heritage Conservation Area, to the west by Burwood Road which is the boundary with the Burwood LGA, and to the south by the Cooks River which forms a natural barrier. The Northern boundary is a formed by the lots facing Georges River Road, and a combination of Albert Road and the lots north of Picken oval where the existing land zoning has resulted in pattern of medium density developments on larger lots which contrast with the typology and lot patterns within the LCA.

The character area features a gentle slope down from the north of the area which flattens as it reaches the Cooks River at the southern side of the area. The flat nature of the LCA and the wide linear streets allow for views through the public domain to the network of public parks and reserves through the area.

The residential lots tend to be narrow and deep to align with the axial road network and generally feature a lot widths of 10 - 12 m, lots in the east of the LCA can feature frontages of 15m wide. The regular

5.1 EXISTING CHARACTER

subdivision pattern features residential lot sizes which are generally 450 - 500 sqm. There are number of larger lots throughout the LCA which range in size between 510 – 620 sqm. These are often deep lots or lots which deviate from the standard rectangular form. The majority of residential façades have a street setback of between 4.5 – 7m.

The area is characterised as a low density residential area and typology is primarily one and two storey detached housing on single lots. Earlier styles that characterise the area include inter-war double fronted brick houses with verandah and double gable, steeply pitched roofs. In recent years two storey face brick and rendered houses have replaced single storey houses. Other housing types include older semi-detached houses, a small number of two and storey residential 'walk-ups' and some recent dual occupancies, however there are few examples of these types. External materiality is diverse however the range of brick, weatherboard, rendered or painted façades form a cohesive pallet. Newer developments or generally finished in subdued colours that reflect the existing character of the area.

Front boundaries are typically demarked by open and low front fencing. Front setbacks often feature landscaped front yards. These contain small areas of lawns and gardens planted with low scale native and exotic species which contribute to the amenity and sense of open space on the street. Most properties feature single driveway crossovers and open carparking structures located in front of the building and garages are common. However despite the amount of private parking provision, the public streets are used extensively for parking of cars and other vehicles such as boats. This results in some streets becoming congested.

The area includes multiple heritage items which contribute to the character of the area. These the former St John's Roman Catholic Church on Cook Street, a Victorian Shop with a dwelling above on Balmoral Avenue, two pairs of Victorian semi-detached houses on Windsor Avenue and a Victorian house on Hampton Street.

Croydon Park is characterised by its large number of open spaces provide the majority of tree stock in the area. Planting is generally native trees which provide areas of extensive canopy, although some exotic species have been planted. The network of parks and reserves support a range of recreational activities. Sando Reserve, Balmoral and Broad Street Reserves are more naturalistic parks whilst Rosedale Reserve, Croydon park feature sporting fields and Picken Oval and Magpie sports club provide opportunity for formalised club sport. The significant Cooks River and Cycleway park forms the southern boundary of the area.

The character area does not contain any retail, however the Small Village Centre on Georges River Road is in close proximity to the LCA and provides a wide range of services for the area.

There are a range of community facilities in the area including the Korean Society of Australia, the Campsie Chinese Congregational Church. There are no schools within the LCA however there are two privately run childcare centres. The Magpie Sports Club is the primary entertainment venue offering a range of recreational activities and spaces for hire.

Burwood Road and Brighton Avenue are the major connecting roads which link to Georges River Road to the north, and crosses the Cooks River providing access to the suburbs to the south. The road network within the LCA is generally rectilinear providing a clear hierarchy of streets. The area is reasonably well served by Public transport. The bus stops on Burwood Road and Brighton Avenue provide regular services from within the LCA to Campsie Train Station and Campsie Local Centre where bus services the international and domestic airport terminals can be accessed.

The streets are generally wide which provide sense of open space and include good quality footpaths and width verges found on most street which support native street tree planting at a range of scales, however the quality of the tree planting is mixed. The area has a high degree of walkability, and it is easy to move between the extensive network of green spaces and small reserves. The Cooks River Cycleway at the south of the LCA is a key feature of the area which provides a connection to the surrounding suburbs and access to Sydney's growing cycling network.

5.1 EXISTING CHARACTER



Balmoral Avenue is typical of the area being a rectilinear wide street lined with grassed verges and footpaths to both sides of the street. Street trees are a range of native species providing varies levels of sun protection. North - south streets typically terminate at the southern boundary of the character area providing views to the reserves and public recreational spaces which characterise the area.



The scale of the area is characterised by one and two storey built forms. Newer two-storey residences of a range of architectural styles sit alongside single storey residences which tend to be older inter-war or bungalow style buildings featuring weatherboard façades.



An example of single storey bungalow housing alongside a newer two-storey development both with weatherboard façades and open carports in the front yard. A mature significant tree located in the backyard, and landscaped front gardens contribute to the quality of the landscaped setting



Existing bungalow housing with mid-tone coloured gables, hip and gable roofs in tiled or corrugated finish, open fencing and soft landscaping in the front setback. The façades are weatherboard or a rendered finish and the buildings have been finished in mid-tones.

5.1 FUTURE CHARACTER

DESIRED FUTURE CHARACTER

Croydon Park is a high quality suburban environment which should maintain its existing character. To achieve this the area should remain as low density residential area and the primary development type should be detached dwellings on single lots. Care should be taken when allowing alternate residential building types as the additional density could potentially reduce the amenity of the area. Future development should be guided by the controls within Councils LEP and DCP. This will ensure the outcomes are compatible with the existing character of the area.

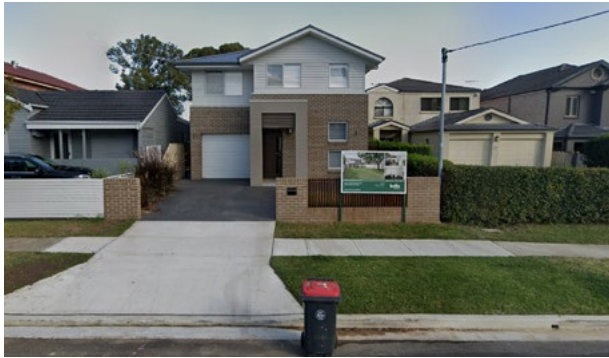
Future residential development should have a maximum building height of two storeys and feature side setbacks which will ensure building separation and minimise bulk and scale. Front setbacks should be consistent with the existing setbacks on the street. Front fencing is to be low and open. Front yards should feature soft landscaping , and native tree planting should be encouraged to enhance the streetscape and improve microclimate.

Driveways should be single crossovers to minimise areas of hard landscaping. Where possible they should be located to retain on-street parking and to maximise areas for grassed verges and street tree planting. Garages are to be located behind the front building and far enough from the front boundary to accommodate a parked vehicle.

New buildings should feature modulated façades, incorporate shade elements, and avoid large areas of monotonous surface to reduce apparent bulk. Colours and materials should be selected to minimise the urban heat island effect, and to complement the existing character of the area.

The high quality street, pedestrian and cycling infrastructure should be maintained. Opportunities to enhance the streetscape by ensuring street tree planting is an appropriate native species, and by increasing the number of street lights, should be pursued by the relevant authority.

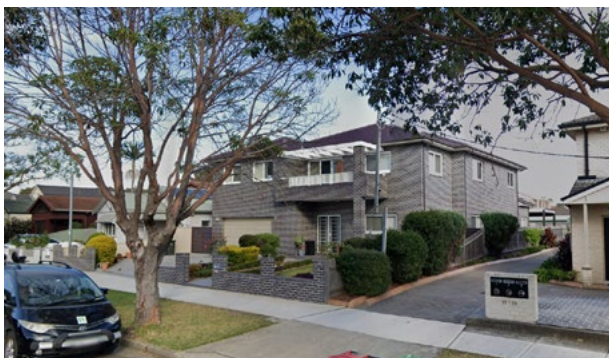
5.1 FUTURE CHARACTER



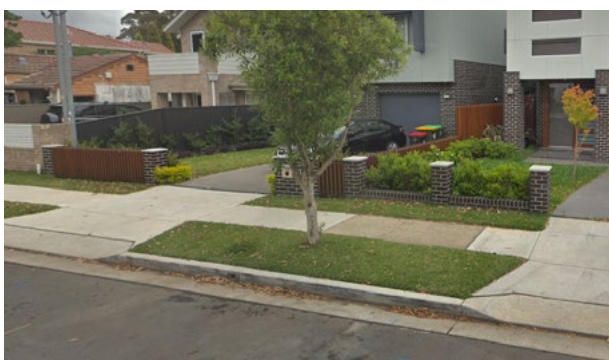
New residence with a single driveway and a garage setback behind the front façade, a hip and gable roof and façade materials sympathetic to the existing character



An existing bungalow housing which have been finished in light and mid-tone colours but now feature some contemporary fittings and fixtures.



Example of a recently completed residence with brick façade, tiled hip roof, low fencing and a landscaped front setback reinforces the character of the open streetscape



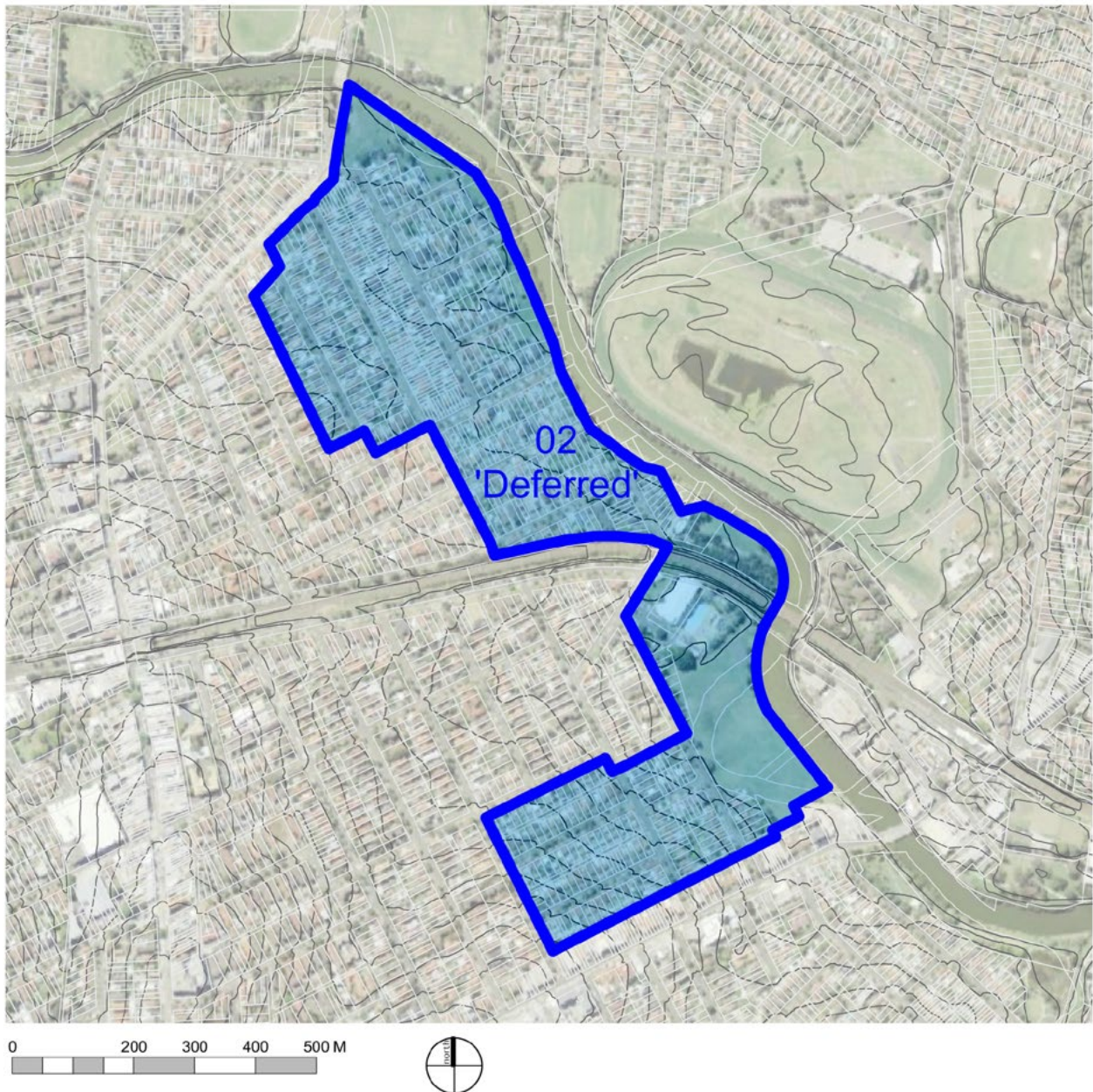
Example of a recently completed detached dual occupancy development with low front low fencing, single driveways and landscaped front setback

5.1 RECOMMENDED PROVISIONS

OBJECTIVES AND CONTROLS

Objective		Recommended Controls	
O1	Maintain the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
		C2	Minimum lot size is to be 450 m ²
		C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
		C4	Minimum front setback is to be 5.5m
		C5	Maintain residential side setbacks consistent with the established street pattern.
		C6	Minimum side setback to be 0.9m clear of obstructions
		C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
		C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
		C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
		C10	Maximum height of front fence to be 1.2m
		C11	Provide street trees of an appropriate native species and increased in numbers.
O2	Maintain the low scale of the LCA.	C12	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O3	Maintain the articulated built form which characterises the area.	C13	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
		C14	Provide a medium pitched primary roof form consistent with the area.
O4	Maintain a cohesive palette of materials and colours.	C15	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
		C16	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
		C17	Provide materials and colours that are light or mid-tone and complementarity with the existing finishes in the area.
O5	Minimise the visual impact of off street parking on the streetscape.	C18	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision
		C19	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
		C20	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

5.2 LOCAL CHARACTER STATEMENT (DEFERRED)



CAMPSIE AND CANTERBURY EAST (DEFERRED)

The Campsie and Canterbury Local Character Area is located in the east of the LGA Adjacent to the Cooks River. The LCA encompasses the east of the suburb of Campsie and it crosses the T3 Bankstown trainline to incorporate a small part of the Canterbury. The northern boundary of the LCA is formed by Brighton Avenue and Mildura Reserve. The south is bound by the site facing on to Canterbury Road. The Cooks River to the east forms a natural boundary which separates the LCA from Ashbury and the Canterbury Racecourse. The western boundary flows Burns Street and Moore Street in Campsie, and Phillips Avenue and Gould Street In Canterbury. These streets mark a clear delineation between the low scale suburban residential character within the LCA and the mid to high density developments which characterise the higher density land zoning to the west of the Character Area.

The topography of the area features a fall from the west of the LCA to the east where it flattens out on the blocks adjacent to Tasker park and the Cooks River. Although the slope is slightly more pronounced in the south of the LCA, the falls are generally moderate. A regular axial network of streets has been established within the LCA, with rear lanes to the blocks on Redman, Waratah, and Emu Street.

5.2 EXISTING CHARACTER (DEFERRED)

Lot sizes are narrow with the majority being around 12m wide. The larger lots of 500 - 600 sqm are predominantly located in the east of the LCA, the western side of Burns Street and adjacent the southern boundary. Lots between Moore Street and Burns Street sites are consistently around 490 sqm in size. The lots between Redmond Street and Emu Street are approximately 450 sqm in size.

Some areas within the LCA feature narrow and smaller lots. In the south the 8m wide, 290 sqm lots located between Ward Street and Warrigal feature older single dual occupancies. However the scale and larger side setbacks present to the street as single developments on wider lots. In the north of the LCA the lots between Moore Street and Cowper Street consist of historic narrow 6m wide lots of between 220 - 230 sqm in size. Whilst some small houses remain on these lots, the majority have been developed with a single dwelling built across two lots creating an impression of a less fine lot pattern consistent with the larger lots located within the LCA.

Front setbacks vary depending on lot width and street configuration, although the front façades are generally located 5m-6m from the street boundary. The properties with rear lane access and the lots between Cowper Street and Moore Street feature smaller setbacks of 3m-4.5m.

The area is low scale, generally consisting of one and two storey residential buildings, however there is a small area of manufacturing near the centre of the LCA. The housing styles in the area are diverse with no single prevailing architectural style or era. There are no heritage items within the LCA and older housing consists of single storey rendered, fibro and weatherboard clad houses on narrow lots. There are a few examples of bungalows, inter-war brick housing and historic dual occupancies which are predominantly located in the south of the LCA. Late 20th century housing consists of one and two storey detached housing with pitched roofs and brick façades. 21st century development is generally two storey detached dwellings or two storey dual occupancies finished in brick. Many newer developments feature rendered elements in a contemporary colour palette.

Front fencing is consistently low masonry or open with some properties featuring no front fencing. Setbacks often feature small lawns although a number of yards, especially those on narrow lots such as those located between Cowper Street and Moore Street, are dominated by hard surfaces and driveways. Substantial garden planting is common in the south of the LCA and less common in the north where building façades can often be the dominant feature on the street.

Driveway crossovers are a mix of single and double driveways which are often well spaced to provide for grass verges and the like. The blocks with rear lane access and some of the older housing on the very narrow lots do not feature driveways. Offstreet parking provision is a mixture of open and covered parking as well as single and double garaging, on the lots with rear lane access, this garaging is located on the lot line at the rear of the property. Public streets are used extensively for vehicle parking throughout the LCA. This is especially prevalent where on-street parking is the only option for residents creating a car dominated character in the centre of the LCA, and the south where the proximity to Canterbury Road where on street parking provision may already be under additional pressure by non-residents.

The area features formal and informal green open space at a range of sizes. Mildura Reserve is located at the north of the area and it features a play area and open space to support a range of activities. Little Tasker Park features a mix of open space and a dirt bike track interwoven between more naturalistic planting. Tasker park is a very large park with large open spaces which accommodate a range of sporting fields and play equipment. Tree planting in the park is limited to the edges of the park but including a distinct row of palm trees adjacent to the Cooks River. There are several areas of open space adjacent to the Cooks River in addition to the formal Reserves. These informal spaces lack tree planting as they are located under high voltage powerlines, however they are well maintained by council and offer additional opportunity for outdoor activity.

5.2 EXISTING CHARACTER (DEFERRED)

The Canterbury Ice Rink and the Canterbury Leisure and Aquatic Centre located at the northern edge of Tasker Park providing a range of recreational and fitness activities to the residents within the LCA and the wider LGA. There is limited business activity within the area and no retail provision, however the LCA is well serviced by the Campsie Town Centre to the west of the LCA, the businesses located along Canterbury Road to the south, and the Canterbury Local Centre East of the Cooks River. The Campsie Community Church provides religious services. There is a privately run childcare centre located in the north of the LCA, however there are no schools located within the area.

Both the north and south of the LCA have good internal vehicle connectivity, with easy access to the surrounding suburbs from the major roadways located in close proximity to the Character Area. Movement between the north and south of the area is restricted by the T3 trainline, with only a single connection point between the two located where Wairoa Street crosses under the rail line. There is limited public transport in the north LCA, with a single bus stop on Brighton Avenue on the northern boundary providing services to central Campsie however the distance from the centre of the area means it may not be walkable for all residents. The south of the LCA is better serviced with a bus route running through the area providing semi-regular links to Bankstown and the Canterbury Local Centre, although residents are also able to access the large number of bus services on Canterbury Road.

The streets are lined with good quality footpaths. Street trees are native species are generally between 3m-5m in height, although some taller trees exist in the southern area. The density of and species of planting varies by street, and sometimes within a street so the quality of shade coverage is mixed. Whilst the trainline limits connection between the north and south of the LCA elsewhere area is fairly permeable and has a good degree of walkability, however alternates to active transport may be the preferred option to travel between the north and south of the LCA. There are multiple connection points along the eastern boundary of the LCA which provide pedestrian access to the Cycleway and the extended green network of parks and reserves located alongside the Cooks River.

5.2 EXISTING CHARACTER (DEFERRED)



Older residential housing is most often seen in the east of the SCA. These houses feature small landscaped front setbacks and parking at the rear of the property which is accessed via the rear lane.



An example of the typical street which features a mix of older single storey residential housing with open carport alongside a late 20th century 2 storey house.



The scale of the area is characterised by one and two storey built forms. Architecture can vary greatly with no single prevailing architectural style or era. However common elements such as masonry façade, and pitched hipped and gable roofs are common on many houses.



Clissold Parade is typical of the wider streets in the area featuring grassed verges and footpaths to both sides of the street. Street trees are a range of native species providing varie levels of sun protection. The east- west streets typically terminate at the grean spaces lining the Cooks River.

5.2 FUTURE CHARACTER (DEFERRED)

DESIRED FUTURE CHARACTER

The Campsie and Canterbury East Local Character Area should maintain its existing character as a low density residential area. The primary development type should be low scale detached housing within a landscaped setting. Care should be taken when allowing alternate residential building types as the additional density could potentially reduce the amenity of the area. Where permitted new dual occupancies and multi-residential development should be guided by the controls within Councils LEP and DCP. This will ensure the outcomes are compatible with the existing character of the area.

Future development should have a maximum building height of two storeys and should feature side setbacks on wide lots, and where possible on narrow lots, to ensure building separation which will minimise bulk and scale. Front setbacks should be generous to allow for areas of soft landscaping forward of the building line and should contain minimal hard surfaces. Native tree planting should be encouraged to increase the greening of the area and improve microclimate. Where fences are used to delineate front boundaries, they are to have a low and open form consistent with the form of the area.

Driveways should be single crossovers to allow maximum space soft landscaping in the private domain. They should be located to retain on-street parking and maximise areas for grassed verges and street tree planting. Garages are to be located behind the front building line to avoid it becoming the dominant façade feature, and set back from the front boundary to accommodate a parked vehicle to help relieve pressure on the on street parking provision.

New buildings should feature modulated façades, incorporate shade elements, and avoid large areas of monotonous surface to reduce apparent bulk. Colours and materials should be selected to complement the existing character of the area and minimise the urban heat island effect.

The existing vehicle and pedestrian network should be maintained. Opportunities to enhance the quality of the streetscape by increasing the amount of street tree provision, especially in areas which lack planting, should be pursued by council.

5.2 FUTURE CHARACTER (DEFERRED)



New attached dual occupancy development constructed on two existing narrow lots. The design features well spaced single driveway crossovers, garages setback behind the front façade. The hipped roof, façade materials and low and open fence are sympathetic to the existing character.



Example of a two recently completed 2 storey residences featuring modulated façade elements which break down apparent bulk and give depth to the building.



Example of a single storey addition to an existing dual occupancy. The scale, materiality and location at the rear of the property ensures that the addition is sympathetic to the existing residence and the character of the street.



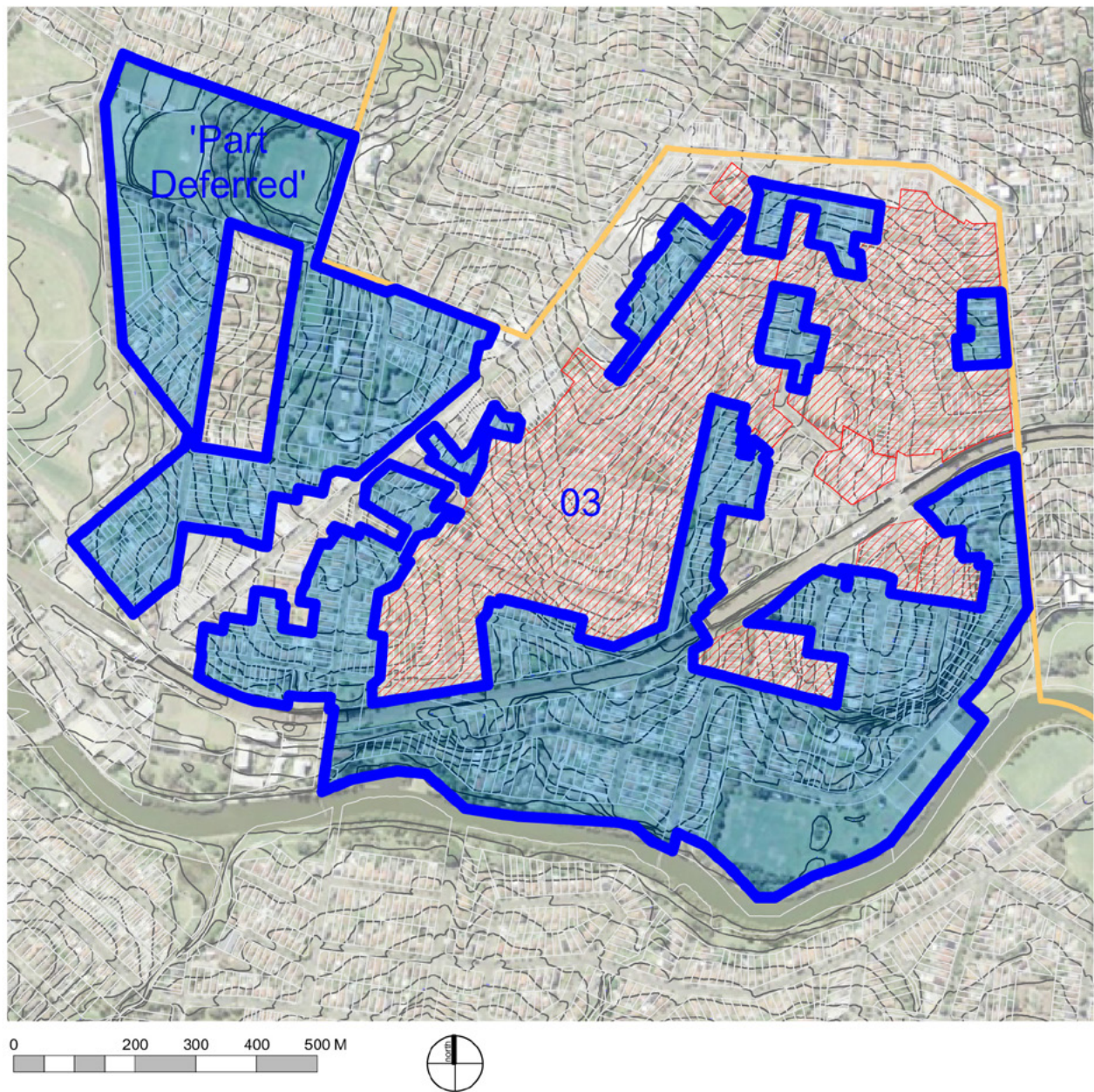
Example of newer single storey development within the LCA which relates to the existing scale, bulk and reflects the architecture style of the adjacent properties.

5.2 RECOMMENDED PROVISIONS (DEFERRED)

OBJECTIVES AND CONTROLS

Objective		Recommended Controls	
O1	Maintain the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
		C2	Minimum lot size is to be 450 m ²
		C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
		C4	Minimum front setback is to be 5.5m
		C5	Maintain residential side setbacks consistent with the established street pattern.
		C6	Minimum side setback to be 0.9m clear of obstructions
		C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
		C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
		C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
		C10	Maximum height of front fence to be 1.2m
		C11	Provide street trees of an appropriate native species and increased in numbers.
O2	Maintain the low scale of the LCA.	C12	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O3	Maintain the articulated built form which characterises the area.	C13	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
		C14	Provide a medium pitched primary roof form consistent with the area.
O4	Maintain a cohesive palette of materials and colours.	C15	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
		C16	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
		C17	Provide materials and colours that are light or mid-tone and complementarity with the existing finishes in the area.
O5	Minimise the visual impact of off street parking on the streetscape.	C18	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision
		C19	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
		C20	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

5.3 LOCAL CHARACTER STATEMENT (PART DEFERRED)



HURLSTONE PARK AND NORTH CANTERBURY (PART DEFERRED)

The Local Character Area is located at the eastern Boundary of Canterbury Bankstown LGA north of the Cooks River. It includes both Hurlstone Park and the northern edge of the Suburb of Canterbury. The area straddles both the T3 Trainline and Canterbury Road and consists of a number of areas which feature similar characteristics but are separated by the Draft Hurlstone Park Heritage Conservation Area.

The west of the LCA is bound to the north by the Ashbury Conservation, the Canterbury Park Racecourse to the west, and the T3 Trainline in the South. The Area excludes a residential block which features a large number of residential apartment buildings which are inconsistent with the building type and scale within the LCA. The east of the LCA is bound to north by the properties facing New Canterbury Road, the south by the Cooks River, and the East by Garnet Street which also forms the boundary with the Innerwest Council LGA.

5.3 EXISTING CHARACTER (PART DEFERRED)

The land falls from the north down to the Cooks River in the south but is characterised by its undulating topography and occasional steep slopes. Streets are generally aligned in response to the landforms and often frame views to the peaks, valleys and points of interest in the area. The changeable elevation across individual allotments has a considerable influence on the siting of each dwelling and in turn the relationship of the building with the street. Houses on the high side of the street often elevated above the public domain and conversely those on the low side are often set lower than street level.

Residential lots range between 400 - 500 sqm in size. This increases to 520 - 650 sqm for some lots to the north of Hurlstone Park, and the lots to the south adjacent to the Train line. The Canterbury side of the LCA contains a mix of these lot sizes. Frontage width is generally 11 - 13m throughout the LCA. The larger lot sizes usually result from increased depth rather than width, creating a consistent pattern of development. Although some lots such as those facing Ewen Park are up to 16m wide. Front setbacks across the LCA typically range from 3 - 5m and front façades are often consistently aligned on each street. Some groups of housing feature setbacks of up to 8m or more, although this is generally due to the existing topography in each property.

The Area is primarily low density residential and the predominant building type is one story detached dwellings on single lots. Housing styles including Federation houses and Bungalows and examples of inter-war housing with tiled or slate roofs. Newer residential development from the late 20th century onwards are generally 2 storey houses on single lots, although there are some examples of 2 storey attached dual occupancies in the LCA. There some mid-late 20th century three storey apartment buildings located within the area, although these are atypical and few in number. Brick is the primary façade finish throughout the LGA. Newer houses often incorporate additional rendered elements and colours, finishes, and fixtures not seen on older housing with care needing to be taken to ensure these elements are compatible with the character of the LCA. Overall the residential architecture in the area has retained most of its rich heritage character with recent examples generally sympathetic to the existing character in scale and form.

Front boundaries are often marked with low masonry brick walls, paling or open fences. Due to changes in elevation many front yards incorporate stairs forward of the building line to link the street to front verandahs. Most lots feature front gardens which contribute to the green streetscape. Sites with more substantial elevation changes often incorporate tiered gardens and can feature substantial retaining walls on the street boundary, although the latter is most common on the properties located near the south-east boundary of the LCA.

Driveways are generally single driveways with offstreet parking commonly front of the building line under open structures. On properties in the west of the LCA driveways are often located to the side of the property and continue to the rear of the property, creating large separation setbacks between houses. Garages are most often seen on sites in the south-east of the area on sloping sites where they are located at or near the front boundary. Some smaller lots do not feature on-site parking which can cause some congestion in areas which feature substantial use of on-street parking.

Although it has been subject to more development than the adjacent HCA, the LCA has retained much of its historic character. This is evidenced through housing styles and streetscape which often seamlessly blend in with the HCA. The area features large Heritage items which contribute to the character of the area including the Victorian school building and the St Paul's Anglican Church in the west of the LCA. A number of additional local heritage items have been nominated, and it is expected these will to be formalised by Council in the near future.

The LCA features several public parks and reserves which vary in size and support range of outdoor recreational activities. The Campbell Athletic Field and Blick Oval are located at the north-west of the LCA and host a range of formal sports. These two locations sit either side of Canterbury Park which features extensive native tree planting. Ewan Park, located at the south-east of the LCA, is an extensive open space consisting of a series of sporting fields and public facilities. A large number of reserves and

5.3 EXISTING CHARACTER (PART DEFERRED)

small parks are located throughout the LCA providing residents with ample opportunities to access outdoor spaces in close proximity to their residences.

The area is characterised by its extensive tree cover. This is a combination of planting in the private domain and the extensive street tree planting which is generally a consistent scale and species. The planting in the public parks is often mature natives at a mix of densities and the Cooks River provides access to a combination of naturalised and formal landscapes.

The area is very well serviced with retail, schools and a range of community facilities within or in close proximity to the LCA. The Crinan Street Local centre adjacent the Hurlstone Park train station accommodates local businesses and shops that service the LCA. Canterbury Road and New Canterbury Road host a diverse range of services and retail options. The Ashbury Senior Citizen's Centre provides a program of community activities. St Pauls Canterbury and St Stephanos Church provide religious services within the area. There are number of childcare centres located throughout the area and within the adjacent HCA. The Canterbury Primary School and Canterbury Girls high School are located within the area, with the Canterbury Boys Highschool just to the North of the LCA.

Canterbury Road intersects the LCA and crosses the Cooks River linking the area to the suburbs of Canterbury, Campsie and Earlwood. New Canterbury Road at the north of the LCA provides access to the Sydney's inner west suburbs. The area is serviced by both bus and train services. The T3 trainline is a distinctive feature of the LCA and the Hurlstone Park station provides access to regular services to the CBD and the Canterbury Bankstown LGA. There are a large number of bus stops within the LCA which provide important links across the area. However the regularity of the services differs across the LCA and some areas are not serviced beyond the early evening.

Within the LCA the network of residential streets have retained much of their existing character preserving a cohesive setting across most of the area. The extensive street tree planting and network of footpaths create a highly walkable environment during the day, however this is offset by a lack of street lighting in many areas. The sloping topography may also make active transport difficult for some individuals who may need to use alternate forms of transport to move through the LCA.

5.3 EXISTING CHARACTER (PART DEFERRED)



Example of a typical grass verge strip, low street tree, and planting in the front yard which combine to create a green streetscape.



Example of high quality bungalow housing with consistent front alignment, low fencing and landscaped front gardens.



Residences on sloped sites featuring areas of stone masonry retaining wall and a garage on the front boundary.



Typical sloping street, featuring extensive street tree planting and space for on street parking which are heavily utilised.

5.3 FUTURE CHARACTER (PART DEFERRED)

DESIRED FUTURE CHARACTER

Hurlstone Park and North Canterbury is an area rich in character and high quality architecture which combine to create an outstanding Local Character Area which should be protected and maintained.

Future development is to be consistent with the existing character. This is primarily low scale detached residential dwelling on single lots, which will reinforce the existing character area. Residential should have a maximum height of two storeys. Side setbacks should be generous to ensure the existing pattern of building separation is maintained. Front setbacks should be consistent with the prevailing street setback. Front yards should and be planted with areas of soft landscaping forward of the building line. Native tree planting should be encouraged to enhance the streetscape and enhance the microclimate. Where fences are used to delineate front boundaries, they are to have a low and open form consistent with the form of the area.

New buildings should feature modulated façades, incorporate shade elements, and avoid large areas of monotonous surface to reduce apparent bulk. Colours and materials should be selected to complement the existing character of the area and minimise the urban heat island effect. Alterations and additions are to be sensitive to the existing built form, limited in bulk and scale, and appropriate to the context.

Driveways should be single crossovers to minimise hard surfaces in front setbacks and maximum space soft landscaping in the private domain. They should be located to retain on-street parking and maximise areas for grassed verges and street tree planting. On flat sites garages are to be located behind the front building line and set back from the front boundary to accommodate a parked vehicle. On sloping sites, where possible, garaging should be located to avoid it becoming the dominant façade feature.

High density development is not consistent with the existing character and they should not be located within Hurlstone Park. Some limited scope for dwellings with a higher intensity may be suitable on the properties to the west of Canterbury road in the suburb of Canterbury. Any future development on these sites is to be guided by the controls within Councils LEP and DCP. This will ensure the built form will be at scale and form appropriate to the context, and the existing amenity will not be diminished.

The high quality vehicle and pedestrian network is of high quality should be maintained. However opportunities to improve connectivity of the area by increasing the regularity and hours of bus services to the more isolated parts of the LCA should be explored.

5.3 FUTURE CHARACTER (PART DEFERRED)



Example of a recently completed residence featuring a tiled roof, brick and weatherboard façade, low fencing in scale and character with the adjacent properties.



Example of a desirable street in the area continuous tree planting and dense canopy.



A recessive 2 storey addition to an existing residence located to the rear of a property.



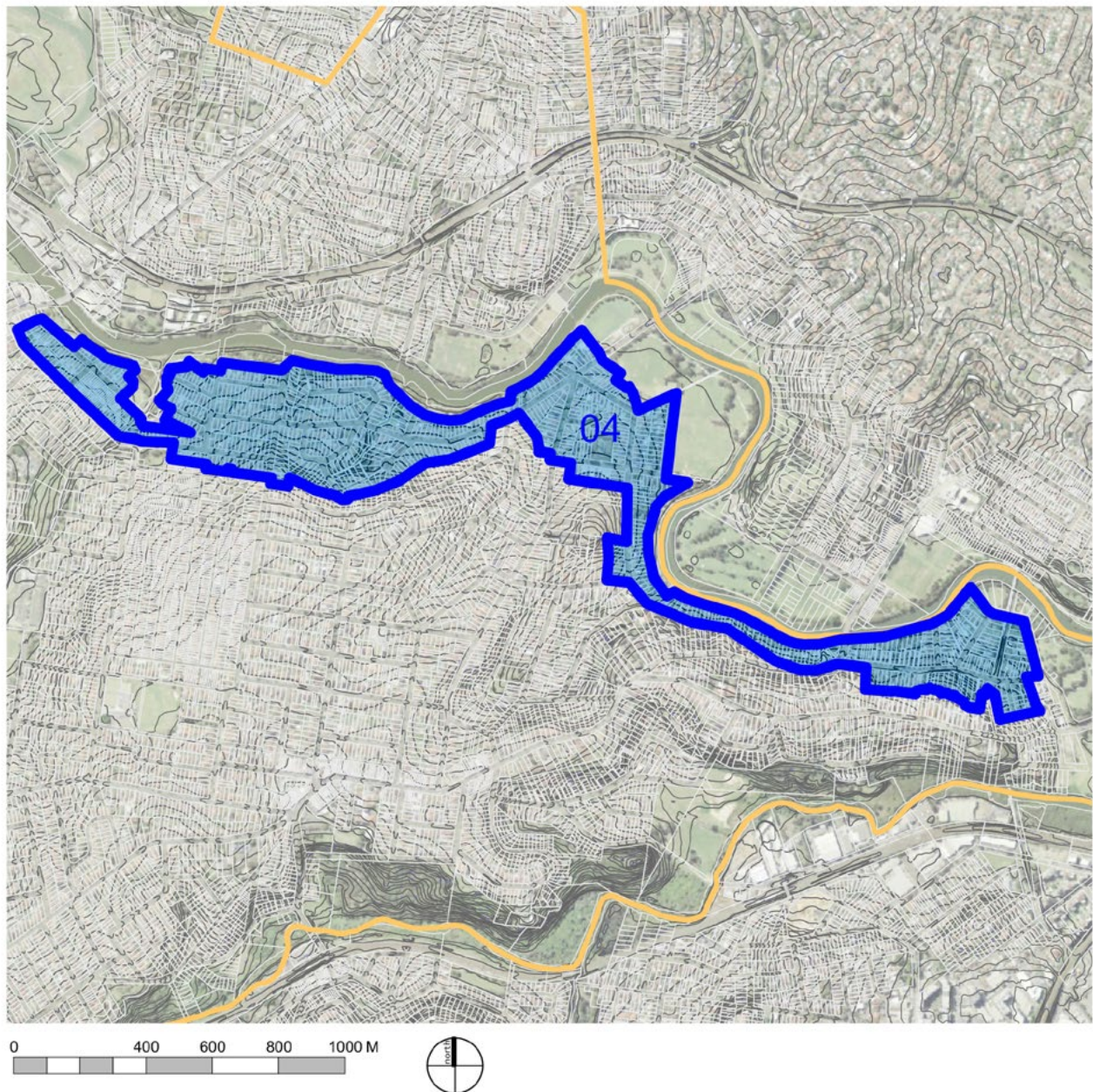
Views to parks and green spaces are an important element of the character area and are to be maintained

5.3 RECOMMENDED PROVISIONS

OBJECTIVES AND CONTROLS

Objective		Recommended Controls
O1 Maintain the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
	C2	Minimum lot size is to be 450 m ²
	C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
	C4	Minimum front setback is to be 5.5m
	C5	Maintain residential side setbacks consistent with the established street pattern.
	C6	Minimum side setback to be 0.9m clear of obstructions
	C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
	C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
	C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
	C10	Maximum height of front fence to be 1.2m
	C11	Provide street trees of an appropriate native species and increased in numbers.
O2 Maintain the low scale of the LCA.	C12	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O3 Maintain the articulated built form which characterises the area.	C13	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
	C14	Provide a medium pitched primary roof form consistent with the area.
O4 Maintain a cohesive palette of materials and colours.	C15	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
	C16	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
	C17	Provide materials and colours that are light or mid-tone and complementarity with the existing finishes in the area.
O5 Minimise the visual impact of off street parking on the streetscape.	C18	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision
	C19	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
	C20	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

5.4 LOCAL CHARACTER STATEMENT



NORTH EARLWOOD AND CANTERBURY

The Local Character Area includes parts of Canterbury and northern edge of Earlwood and it is located in the east of the Canterbury Bankstown LGA. The Cooks River forms a natural northern boundary. The southern boundary includes Fore Street, Burlington Avenue, Dellwood Avenue, Lovat Avenue, Permanent Avenue, Flinders Road, Riverview Road, and Undercliff Road which together mark the transition between the riverbank, and the steep landform of the adjacent areas. This creates a narrow LCA which is bound to the west by Canterbury and to the east by the Parks located on the Undercliffe peninsula.

The LCA falls from it's the high point on the southern boundary down to the Cooks River and surrounding reserves. The rate of slope varies across the area and while extensive groundworks are uncommon the steeper areas have influenced the siting and built form of houses. Housing on the low side of a road does not generally step down in response to the landfall, resulting in elevated rear façades. Conversely the front façade of housing on the high side of street is often raised to accommodate for the elevation at the rear. The areas of deeper lots also tend to be on sloped sites which allow for larger building envelopes, often without a corresponding increase in lot width.

5.4 EXISTING CHARACTER

The road network has developed in response to the topography and the major roads follow the base of the hill at the southern boundary, providing views of exposed sandstone and slope faces leading up to the ridgeline above the Area's southern boundary. Secondary roads generally terminate at the public parks and reserves, providing views to the green network.

Residential lots are generally narrow and deep, varying in shape to match the road pattern and the natural features of the LCA. Lot sizes generally range between 500 - 650 sqm although there are some variations, lots in the west tend to be 400 - 450 sqm and there is a small area of 6 m wide 245 sqm lots in Canterbury. Larger lots of 700 - 800 sqm are not uncommon, although the increased size is generally due to lot depth. Lot widths are generally 11 - 14 m at the building line, with the main variance being the series of battle-axe lot east of the Illawarra Road bridge. This has resulted in a consistent pattern of development throughout the LCA despite the variations in lots sizes. Setbacks in the area range from 3 - 5m in Canterbury and 5 - 6m in Earlwood and housing is generally with the prevailing setback on a street. However some lots can feature much larger setbacks of up to 11 m although this is generally due to the shape and relationship of a lot with the street, and compliance with existing frontage alignment.

The LCA has been developed over a number of decades and as a result it displays a range of architectural styles and detailing. The built form of this low density residential area is one and two storey detached dwelling. While some streets feature recurring elements such as gable roofing, period of development or number of storeys there is no single prevailing style across the LCA. The materials palette varies between buildings and generally reflects the period of development. However the widespread use of masonry brick façades and tiled roofs is common thread which characterises the area. The newest buildings are more likely to incorporate additional finishes or non-traditional built forms. Whilst the outcomes are generally compatible with the character of the LCA, care needs to be taken as some elements such as painted rendered façades do not always sit well within the context.

Despite the changeable topography and range of development periods the front fencing and walling is almost uniformly low and open. The materiality and design generally reflect the architectural style of the corresponding property. Front yards generally include small areas of lawns and gardens with low planting of exotic and native species. Most properties feature single driveway crossovers. On older housing the driveway is often to the side of the building and runs to the rear of the property. Off street parking can take the form of open carport structures or uncovered driveway, with garaging less common. On more recent developments, driveways are more likely to be located in front of the building and garaging can then become a prominent façade feature. On street parking is common throughout the area although it appears to be more extensive on the busier roads.

The LCA includes a range of heritage items including the Aboriginal art site and midden on Undercliffe Road is an importance piece of indigenous and state heritage. The Saint Mary Mackillop Reserve and the Cooks River Aqueduct are the largest Heritage items within the LCA and the Interwar Street Trees on Lovat Place contribute to the character of the area. Residential heritage items include the "Iserbrook" Federation house on Fore Street, the Victorian house on Riverview Road, the Interwar Stone house and the Federation sandstone shop with attached house which are both located on Undercliff road.

The Cooks River Cycleway runs across most of the northern boundary of the LCA, providing regular access to the Cooks River waterway and the natural environment. The cycleway features almost continuous tree coverage, and it links together a number of reserves, parks and playfields bookended by Gough Whitlam Park in the east, and the historic St Mary Mackillop Reserve at the west. The open spaces are a mix of naturalised and formal landscaping which support a range of outdoor recreational from informal and passive outdoor activities through to exercise and formal club sport.

5.4 EXISTING CHARACTER

The area's narrow shape and limits the potential for community services however there are a small number within the area. These include the Imam Husain Islamic Centre, the Church of Christ Earlwood and the United Pontian Benevolent Brotherhood of NSW. The Greek Orthodox Community Home provides accommodation for the aged. The Earlwood Children's centre is the sole childcare centre within the LCA and there are no schools. However the Canterbury South Public School and the Undercliffe Public School are located a short distance south of the LCA.

There are a small number of specialist businesses in the area however they are located on residential properties, and do not feature commercial frontages. There is minimal retail within the LCA and residents will need to travel out of the LCA to access retail, business services and hospitality outlets. The nearest are the Canterbury Commercial Centre at the east of the LCA, and central Marrickville located a short distance by vehicle to the north of the LCA.

Vehicle connectivity within the area is good as the southern boundary of Fore Street, Burlington Avenue, Permanent Avenue, Flinders Road, Riverview Road, and Undercliffe Road, form a comprehensive road network linking the area. There are multiple points of entry distributed across the LCA providing access from Canterbury, the Innerwest suburbs north of the LCA and the wider suburb of Earlwood. Secondary residential streets have low traffic volumes and generally terminate at areas of public open space.

The level of public transport service within the area is mixed. Fore Street and Permanent Avenue have multiple bus stops and regular services connecting into the wider public transport network. However some residents on Riverview Road and Undercliff Road have to travel upwards of 600 m in order to access bus services with the sole bus stop in the east of the LCA on Homer Street just to the south of the Illawarra Road bridge.

The Cooks River cycleway is a well maintained link which provides a safe environment for walking and cycling, and the series of pedestrian bridges across the Cooks River provide access to areas north of the LCA. Footpaths are common and in reasonable condition, however the quality of the street trees is mixed or even absent in some areas. This is often on the busier roads where pedestrian exposure to vehicle traffic may make walking undesirable, especially over longer distances or areas of changing topography.

5.4 EXISTING CHARACTER



Typical single storey bungalows featuring tiled roofs, consistently aligned front setbacks and landscaped front yards with low and open front fencing.



The natural rock outcrops located along sections of the northern boundary of the LCA are a unique characteristic of the streetscape.



The area features a mix of one and two storey residences built over number of eras, however the widespread use of tiled roofs is common thread between most houses.



Cohesive built forms of gable fronted brick interwar houses with consistent front setbacks and landscaped front yards.

5.4 FUTURE CHARACTER

DESIRED FUTURE CHARACTER

The Canterbury and North Earlwood Local Character Area should maintain its character as a low density residential area. To achieve this the area should remain as low density residential area and the primary development type should be detached dwellings on single lots. Care should be taken when allowing alternate residential building types as the additional density could potentially reduce the amenity of the area. Future development should be guided by the controls within Councils LEP and DCP. This will ensure the outcomes are compatible with the existing character of the area.

The maximum residential building height should be kept to two storeys however this may not be possible on sites with challenging topography. Development on sloping site should be designed to step down or across the landform to lower bulk and scale. The pattern of side setbacks will be retained to ensure separation between. Front setbacks should be generous and consistent with the prevailing street setback. Front fencing will be low and open to reinforce the open nature of the streets. Yards should feature soft landscaping and native tree planting should be encouraged to improve microclimate and tree coverage through the area

Driveways should be single crossovers to minimise hard surfaces in front setbacks and maximum space soft landscaping in the private domain. They should be located to retain on-street parking and maximise areas for grassed verges and street tree planting. On flat sites garages are to be located behind the front building line and set back from the front boundary to accommodate a parked vehicle.

New buildings should not feature large unbroken areas of monotonous materials, and should incorporate feature to reduce apparent bulk and scale such as modulated façades and shade elements. Colours and materials should be selected to minimise the urban heat island effect, and to harmonise with the existing character of the area.

Streets should continue to be well maintained however they will benefit from provision of additional streetlighting. Although public transport should potentially be expanded to cover the eastern edge of the LCA.

5.4 FUTURE CHARACTER



New two storey residence with an open front garden and single garage setback from the front façade. Multiple façade elements create a modulated building form



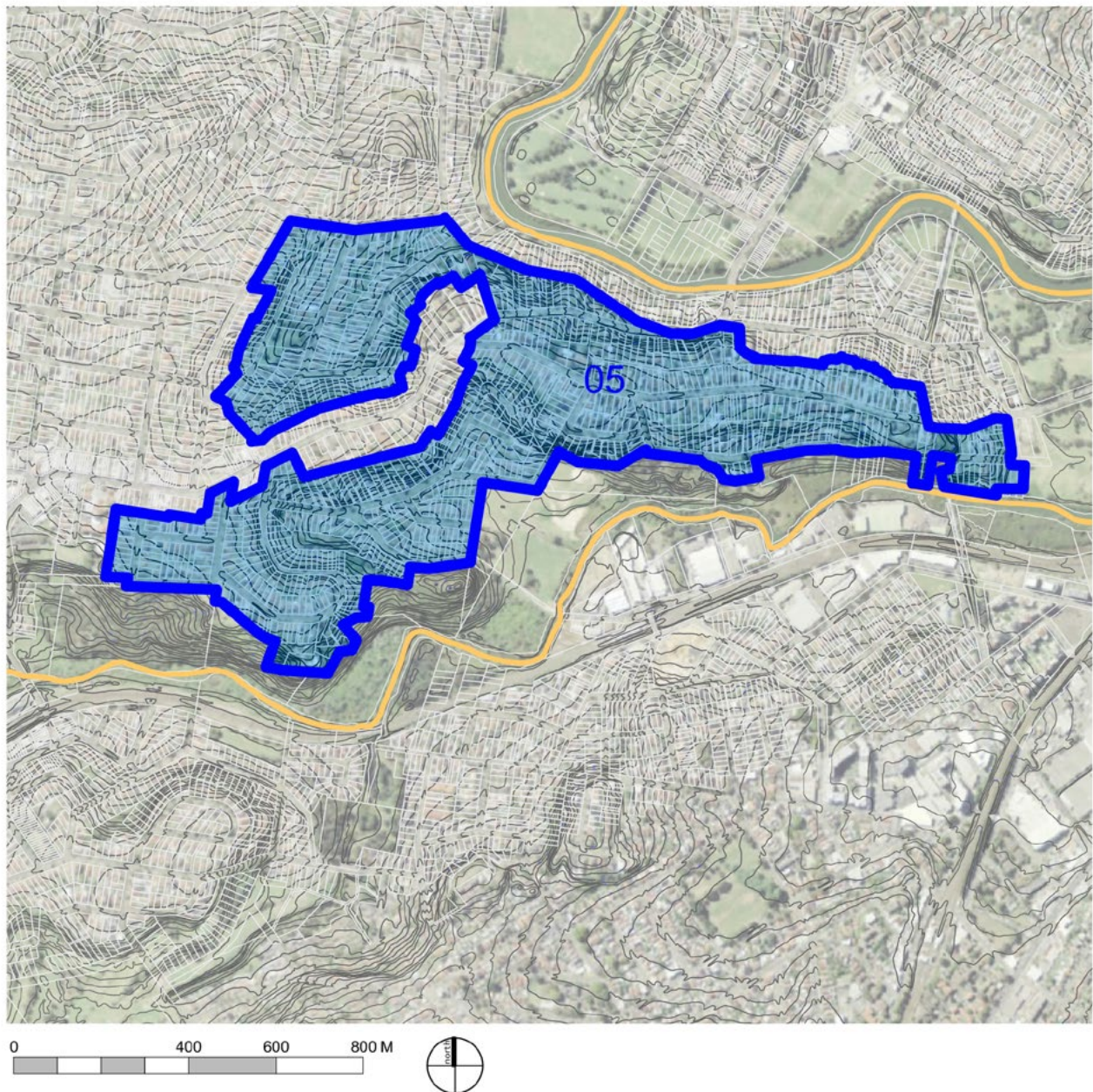
Views to and through the network of green spaces are an important characteristic of the area and they should be protected and maintained.

5.4 RECOMMENDED PROVISIONS

OBJECTIVES AND CONTROLS

Objective		Recommended Controls
O1 Maintain the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
	C2	Minimum lot size is to be 450 m ²
	C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
	C4	Minimum front setback is to be 5.5m
	C5	Maintain residential side setbacks consistent with the established street pattern.
	C6	Minimum side setback to be 0.9m clear of obstructions
	C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
	C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
	C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
	C10	Maximum height of front fence to be 1.2m
	C11	Provide street trees of an appropriate native species and increased in numbers.
O2 Maintain the low scale of the LCA.	C12	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O3 Maintain the articulated built form which characterises the area.	C13	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
	C14	Provide a medium pitched primary roof form consistent with the area.
O4 Maintain a cohesive palette of materials and colours.	C15	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
	C16	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
	C17	Provide materials and colours that are light or mid-tone and complementarity with the existing finishes in the area.
O5 Minimise the visual impact of off street parking on the streetscape.	C18	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision
	C19	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
	C20	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

5.5 LOCAL CHARACTER STATEMENT



EAST EARLWOOD

The Local Character Area is located on the eastern edge of the Canterbury Bankstown LGA in within the suburb of Earlwood. The area is bound to the east by Bamboo Avenue and Waterworth Park. The western boundary is formed in part by Wardell Road and View Street. The northern boundary of Riverview road and Undercliff Road mark the transition from the steeper slopes of the LCA with the flatter land adjacent the Cooks River. The southern boundary is formed by a network of large parks and naturalised bushland which follow the Wolli Creek line and mark the boundary to the Bayside Council LGA. Homer Street separates the north and south of LCA as the street has been developed with a large number of apartment buildings which are not consistent with the character of the residential buildings within the LCA.

The locality is primarily characterised by its setting of steeply sloping topography which falls away from the ridgeline on the peninsula and the series of peaks throughout the LCA. Residential lots are generally located at the top of the slopes and on the ridgeline, although they also line the valley adjacent to the Turella Reserve. The road network generally follows the topography which results in variable street widths and irregular blocks and lot shapes. The ridge is an area of relatively flat land however most lots

5.5 EXISTING CHARACTER

are subject to a degree of slope or cross fall. The topography has a substantial impact on the built form within the LCA which is most evident on steeply sloping land or land featuring natural constraints such as rock outcrops. This dictates a range of design elements including siting, setbacks, building height, and offstreet parking. These in turn impact the buildings' relationship to the public domain with buildings on the high side of the street often visually prominent at close proximity and also when seen from the wider LCA.

The changeable topography allows for views to, from and across the area. Views of the ridgeline, high points and slopes can be obtained throughout the LCA and from the surrounding area. Residential buildings are often a visually prominent feature within the landform. Panoramic views of Wolli Creek and the slopes of Bardwell Park can be obtained from the east and south of the area. Views across the inner west suburbs towards the Sydney CBD can also be obtained from limited locations within the north of the LCA.

Due to the topography and the irregular road network the area contains a large number of irregular and battleaxe lots. Residential lot sizes are inconsistent across the LCA although large lots of between 600 - 850 sqm in size are the most common. Very large and residential lots of 900 sqm are not uncommon although these are generally rectilinear lots on steeply sloping sites. There are a large number of smaller lots between 400 - 500 sqm. These can be the result of subdivisions from dual occupancy development or being located in front of battle axe lots, or sometimes these are a result of the street network constricting lot sizes. There is a smaller number lots of 500 - 600 sqm which are distributed throughout the LCA. These are generally located amongst larger lots or may be an irregular shape such as a battle-axe lot. Lot widths are generally 12 - 14 m in the north-west and south-west of the LCA. The remaining larger single lots in the east of the LCA are generally 15 m wide. Many of these lots, especially those facing Bayview Avenue, have been subdivided into battle axe lots setback from the street or to accommodate attached dual occupancies. Wider lots of up to 20 m exist in the LCA, although these are often on the smaller sites located within the Wolli valley.

Buildings within the flatter areas of the LCA and those constructed on rectilinear lots generally feature front setbacks of 4 - 7 m. Front setbacks distances are strongly influenced by the topography and lot pattern resulting in a large degree of variance across the Area. On sites where the landform dictates the siting, setbacks can be up to 13m, however properties on the high side of a street can feature tall retaining walls and garaging on the boundary creating a nil front setback and continuous street walls in some instances.

The area is characterised as a low - medium density residential area, however detached dwellings are the predominant building typology. The area contains a small number of dual occupancy and multi-residential developments., although these are generally on the lots south of Bayview Avenue. Buildings are generally 1 - 2 storeys although the ratio varies across the LCA. There are a number of 3 and 4 storey developments within the LCA. These are generally only found on steeply sloping lots where buildings have not been designed to step or follow the landform, resulting in very prominent built forms.

The majority of residential buildings feature pitched roofs and masonry façades. However there is a diverse range of architectural styles and materiality within the LCA. This results in an eclectic residential character with limited unifying characteristics. A large number of newer detached residences and dual occupancy developments feature non-traditional design elements such as low profile roofing, extensive parapets, prominent garages and boxed balconies. These elements tend to create visually prominent built forms, although older residences with large façades or uniform materiality can also appear to have excessive bulk. Care needs to be taken to ensure buildings are not intrusive within the setting, especially on sloping sites which elevate and accentuate building height.

There are several contributory items which add to the character of the area. Notable examples of residential heritage are the grouping of 5 Interwar houses on Hocking Avenue, and the group of 5 Federation Stone Houses on Jackson Place. The Victorian era Wolli Creek Aqueduct and the Girraheen Park Gates are items which speak to the historic development of the area.

5.5 EXISTING CHARACTER

The South of the LCA is bound by the Wolli Creek regional park which is made up of Girrahween Park, Nanny Goats Hill and Turella Reserve and Wolli Bluff. It contains a series of open spaces and extensive areas of bushland and features a network of walking tracks which form part of the Two Valley Trail. Waterworth is a large park located on the eastern boundary of the LCA which provides opportunity for formal sport as well as supporting passive activities adjacent the banks on Wolli Creek. A small number of small parks and reserves are located within the LCA including McPherson Reserve in the Northwest, the Arncliffe Road Playground and Albert Park which are both located in the south of the Area.

Front fencing is typically low and open across the LCA, although some areas with steep topography feature more extensive use of tall retaining walls and garaging on the boundary line. Front setbacks normally feature soft landscaping comprised of areas of lawn and mixed planting, and tiered gardening on sloping sites, although it is not uncommon for properties to feature large areas of hard surfaces to accommodate driveway access and off-street parking. This is most often seen on properties located on the low side of the street and on the elevated battle-axe lots.

Single driveway crossovers make up the majority of driveways in the LCA, though double driveway crossovers are often seen on properties located within the south and east of the LCA. On older residences driveways are often located to the side of the lot and run to the rear of the property. Driveways on newer housing generally stop at the building line. Offstreet parking generally takes the form of garages which are often prominent on the façade, however open carports or uncovered areas common on older properties. On street parking is used extensively throughout the area, this can be problematic for vehicular circulation on the narrow sloping streets where there is limited room to manoeuvre.

Retail and Businesses within the LCA are limited and residents will need to travel to the Earlwood Local centre for retail, shopping, business and entertainment opportunities. The Area does not contain any childcare centres however the Undercliffe Public Primary School is located within the LCA, and Our Lady of Lourdes Catholic Primary school is located adjacent the Western Boundary. The Greek Orthodox Parish of the Transfiguration of Our Lord and the Earlwood Anglican Church are located within the LCA and there are a number of places of worship for different Christian denominations the west of the Area. The Chow Cho Poon Nursing home is located adjacent the Homer Street boundary at the centre of the LCA.

The major roads in the LCA are part of a well connected road network with Wardell Road providing access to the Inner west, north of the Cooks River. Bayview Avenue connects the LCA to Tempe and links the east of the site to Homer street which leads to Earlwood Local Centre adjacent the western boundary of the LCA. It also connects the Upper levels of the LCA to the lower land to the lower land adjacent the Cooks River and to Marrickville. The topography and landform strongly influences the location and form of the secondary streets. As a result the vehicular interconnectivity within the area is low and street width is inconsistent, often with wide and narrow streets in close proximity.

Public Transport is serviced regularly with access to the CBD and the surrounding suburbs within the municipality. However bus stops are limited to Homer Street and Wardell Road. These are not walkable from the east of the LCA due to a combination of distance and topography. Turrella Train Station adjacent the southern boundary can be accessed via the pedestrian paths at the bottom of Finlays Avenue and Arncliffe Road however this station is not walkable from most of the LCA.

Streets generally only have a footpath on one side and the condition of the footpaths is mixed. The variable street width means that grass verges vary in size and some streets have footpaths hard up against the road and front fencing. When combined with the sloped land this can create a pedestrian environment that is challenging to navigate. Street trees are a mix of native species which differ across the area and sometimes within streets. Some streets lack regular planting, or cannot support planting due to the topography, so sun protection for pedestrians is inconsistent across the LCA. There are a number of paths within the LCA which link high and low areas which dramatically increases the permeability for pedestrians. This includes Bayview Lane which links the LCA with the low lying land adjacent the Illawarra Road, and the pedestrian path which linking the upper and lower sections of Wavell Parade. The walking tracks within the southern reserves and parklands extend the pedestrian network, however these routes function more for exercise purposes.

5.5 EXISTING CHARACTER



Architecture can vary greatly across the LCA and many areas do not feature a prevailing architectural style or era. The scale of the LCA is characterised by one and two storey built forms. However the topography often has a substantial influence on the relationship of the building with the streetscape. On sloping sites this can result in building with much on a taller built forms, especially on properties with garages located in front of the building, on the lot line.



The characterised by its steeply sloping topography which allows views across the area. Many residences are located on the steep slopes within the SCA, especially around the Wolli Valley. These can become visually prominent elements which stand out amongst the treeline.



The scale of the area is characterised by one and two storey built forms. Architecture can vary greatly with some areas single prevailing architectural style or era. Newer architectural examples may feature non-traditional forms such as low profile roofing which can contrast against the more traditional hipped and gabled roof forms.



Some sections of single storey bungalow housing with hip and gable roofs in tiled finishes, open fencing and soft landscaping in the front setback have been retained. These are often contrasted against newer developments with a larger built form and height, such as those which line the slopes in the background of this image.

5.5 FUTURE CHARACTER

DESIRED FUTURE CHARACTER

The East Earlwood Local Character Area should look to maintain its character as a low density residential area within a landscaped setting. Future development needs to be compatible with and respectful of steeply sloping topography and extensive areas of tree coverage which characterise and define the area.

To achieve the primary development type should be low scale detached housing on single lots with an emphasis of the landscaped setting. Care should be taken when allowing alternate residential building types as the additional density could potentially reduce the amenity of the area. Future development should be guided by the controls within Councils LEP and DCP. This will ensure that any future development is compatible with the existing character of the area.

The maximum residential building height should be kept to two storeys however this may not be possible in areas featuring steep slopes. Development on these properties should be designed to respond to the landform by stepping down or across the slope and should minimise site disturbance. This approach will greatly reduce excessive building height and minimise bulk. This will help to ensure that development sits within the tree line, limiting the potential for it to become an intrusive visual element in the landscape.

Generous side setbacks are to be encouraged to ensure separation between buildings. Front setbacks should be consistent with the prevailing street setback. Yards should feature soft landscaping and native tree planting should be encouraged to improve microclimate and enhance the existing tree coverage in the area. Front fencing will be low and open to reinforce the open nature of the streets.

Driveways should be single crossovers and narrow to minimise the prominence of the driveway in the street. Front setbacks should maximise space soft landscaping in the private domain. They should be located to retain on-street parking and maximise areas for grassed verges and street tree planting. Garages are to be located behind the front building and set back from the front boundary to accommodate a parked vehicle. Where this is not possible, such as on sites with steeply sloping landforms, garaging should ideally be designed and located so it is a recessive element although this should not add undue height to the building form.

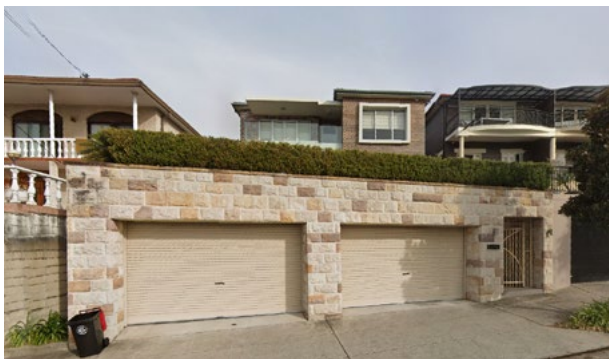
Colours and materials should be selected to minimise the urban heat island effect and harmonise with the natural environment. Large areas of unbroken monotonous materials are to be avoided; and new buildings should incorporate features to reduce apparent bulk and scale such as modulated façades and shade elements.

Streets should continue to be well maintained however they will benefit from provision of additional streetlighting. Council has the opportunity to improve the level of walkability by enhancing the quality of the existing footpaths and extending the existing pedestrian network with new sections of footpath. However opportunities to improve connectivity of the area expanding bus routes to cover more isolated parts of the LCA should be explored.

5.5 FUTURE CHARACTER



Future residential developments should incorporate modulated façades and shade elements to reduce the bulk of the façade. The use of different materials and finishes, as seen here on the entry portico and the main façade is to be encouraged.



On sloping sites when the garage is located on the front boundary it is to be constructed out of stone that relates to the natural materials in the area.



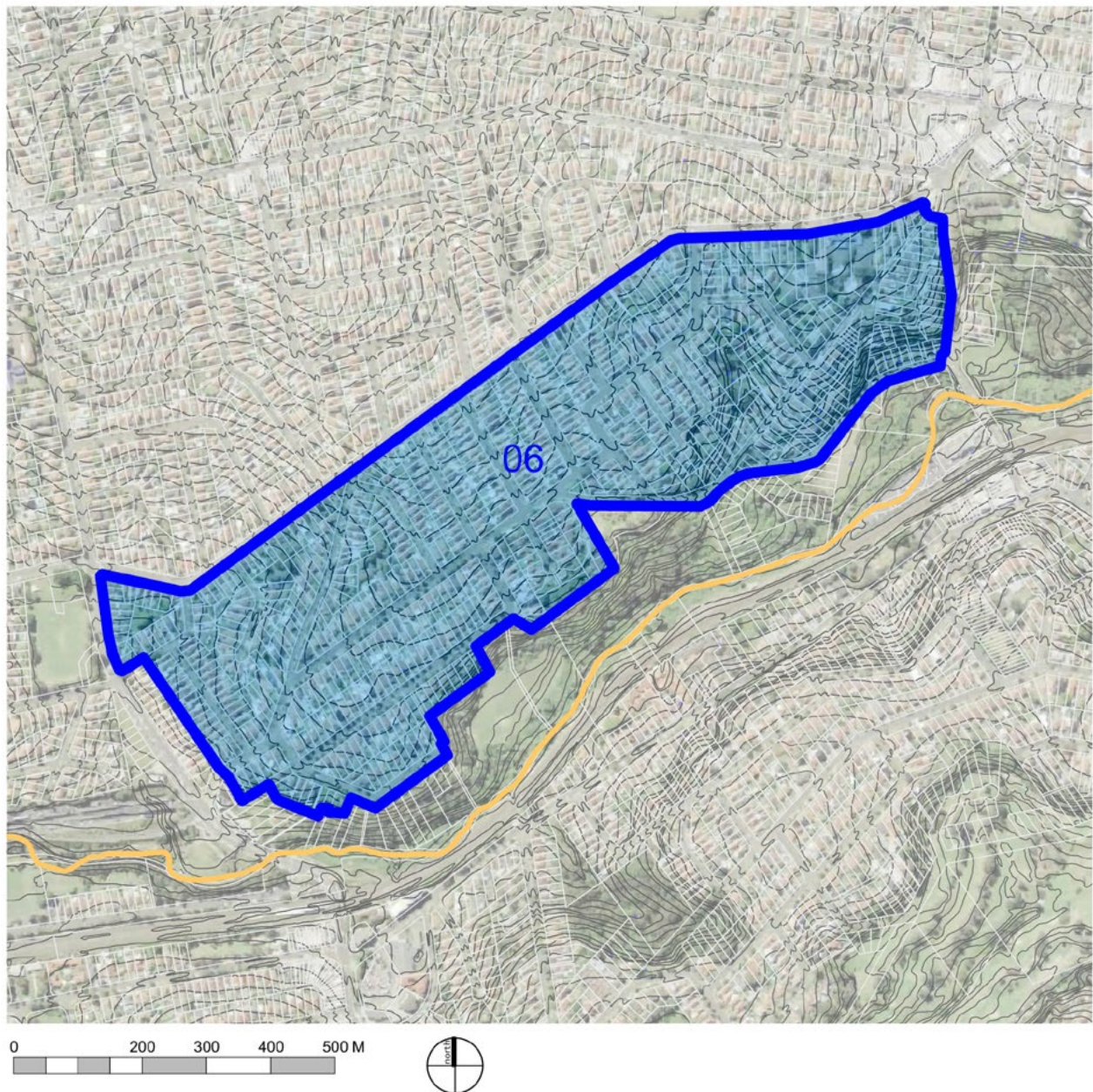
The treelined sloping setting characterises the area. To ensure that residences within the LCA do not become the predominant feature within the landscape extensive canopy tree planting such as those in the centre of this image is to be encouraged as it can help buildings blend into the landscape.

5.5 RECOMMENDED PROVISIONS

OBJECTIVES AND CONTROLS

Objective		Recommended Controls	
O1	Maintain the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
		C2	Minimum lot size is to be 450 m ²
		C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
		C4	Minimum front setback is to be 5.5m
		C5	Maintain residential side setbacks consistent with the established street pattern.
		C6	Minimum side setback to be 0.9m clear of obstructions
		C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
		C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
		C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
		C10	Maximum height of front fence to be 1.2m
		C11	Provide street trees of an appropriate native species and increased in numbers.
O2	Maintain the low scale of the LCA.	C12	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O3	Maintain the articulated built form which characterises the area.	C13	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
		C14	Provide a medium pitched primary roof form consistent with the area.
O4	Maintain a cohesive palette of materials and colours.	C15	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
		C16	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
		C17	Provide materials and colours that are light or mid-tone and complementarity with the existing finishes in the area.
O5	Minimise the visual impact of off street parking on the streetscape.	C18	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision
		C19	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
		C20	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

5.6 LOCAL CHARACTER STATEMENT



SOUTH EARLWOOD

The Local Character Area is located at the southwest corner of the suburb of Earlwood. Homer Street is the Northern Boundary. The eastern and western boundaries are formed by Hartill-Law Avenue and Bexley Road respectively. These roads follow natural valleys which separate the LCA from the adjacent areas. The south of the site is formed by the Illoura Reserve and Wolli Creek which also marks the boundary with the Bayside Council LGA.

The land is located on a plateau above Wolli Creek and it falls towards the south of the LCA from Homer Street. The slope is generally shallow allowing for a mainly rectilinear road network to be formed, however the rate of fall increases along the southern boundary. This is most pronounced at the south east of the LCA where the streets follow the fall of the land, and the steeper slopes strongly influence siting and built form before only being suitable for parkland. Views south across the valley to ridges and slopes of Bardwell Park can be obtained throughout the LCA. Due to the topography the highpoints of LCA are visually prominent when viewed from the surrounding areas.

5.6 EXISTING CHARACTER

The residential lot form is generally narrow and rectilinear in shape, running perpendicular to the street, although lots can vary in shape depending on the topography and road pattern. Lot widths are consistent across the LCA, normally being between 11 - 13 m. Lot sizes are mostly between 400 - 500 sqm however there is variance across the LCA including a small group of lots of 340 - 380 sqm around Bardwell Crescent, and lots of 510 - 600 sqm which are mainly located in the south- west of the LCA. There are a few large lots of 600 - 1000 sqm in size. These lots are typically deep lots which are a result of an angled street alignments, or battle-axe lots. Occasionally the larger lots feature an irregular shape or have wider frontages of 15 -17m although this is rare. Buildings are most commonly setback between 5 - 8 m from the front boundary. However there are some exceptions with some individual dwellings or housing on steeper lots featuring setbacks of up to 11m.

The residential typology of the area is primarily detached housing on single lots and a few examples of low Scale multi-residential developments typically located on battle axe lots. The area has retained many of the inter-war double fronted housing and Californian bungalows and the area is notable for its predominantly single storey built form. There has been a limited amount of redevelopment within the LCA. Although low in number, newer dwellings constructed from the late 20th century onwards are generally 2 storey and occasionally have a 3 storey built form on sloping sites where garages are located below the ground floor. External materiality is predominantly masonry brick although rendered and painted façades are common on buildings from a range of eras. Roofs are generally gable or hip roofing with tiled roof lining which is carried through onto newer development. However there are some exceptions, for example instances of extensive use of parapets contrasting sharply with the surrounding character.

Front fencing is generally low masonry walls although there are instances of open fences or no fencing. Recently completed examples tend to be taller rendered fencing, although these are low in number. Front setbacks generally contain areas of lawns with small areas of additional planting or gardening. Lots with steep slopes such as those on Bray Avenue and the east side of Forrest Avenue feature garaging on the lot line and generally include retaining walls and tiered landscaping before the elevated residence.

Most properties feature single driveway crossovers located to one side of a lot resulting in a pattern of large side setbacks. Private parking is generally uncovered with few examples of carports to the side of housing. Garages are uncommon and generally only seen on newer buildings with smaller side setbacks, or on sloped sites. Due to the spacing of driveways there is a good level of on street parking provision in the LCA.

The Illoura Reserve is a large park with extensive areas of regenerated bushland. It is located on the slopes and valley at the southern boundary where it runs the length of the LCA. The park is flanked by dense regenerated bushland, it features areas of grassed spaces adjacent the banks of Wolli creek and it incorporates part of the Wolli Walking Track. The LCA also contains two small reserves, the SJ Harrison reserve which is adjacent to the Illoura reserve and the Braeside Crescent Reserve which is located in the northwest of the area.

The Area only contains minimal specialist retail and residents will need to travel to the Earlwood Local centre, located a short distance to the north east, to access a wide range of retail, hospitality, and business services. The Area does not feature any pre-school facilities, however the Earlwood Public Primary school is located within the LCA and the Our Lady of Lourdes Catholic Primary school is located adjacent the Earlwood Local centre. The Earlwood Baptist Church provides religious services for the area.

Homer Street links the LCA locally within Earlwood and Kingsgrove to the west. Bexley Road provides a wider link providing a connection to Campsie to the north, and to areas south of Wolli Creek including Bexley and Rockdale. The area is reasonably permeable with multiple entry points into the LCA from the boundary roads and road widths are well sized to accommodate vehicle traffic and on street parking. However there are some narrow streets such as Woodlawn Avenue and Richard Avenue which may be susceptible to congestion due to limited space.

5.6 EXISTING CHARACTER

Bus services operate in Homer Street at the north of the area, providing regular links from the LCA to the Sydney CBD and Campsie. However the bus stops may not be accessible for those with restricted mobility due to distance and the topography from some parts of the LCA. The Bexley North and Bardwell Park train stations to the south of the LCA provide access to the T8 Trainline which links to the Domestic and International Airport Terminals, the Sydney CBD and the Greater Sydney region.

The walkability of the area is mixed due to the variable quality of the street edge combined with the areas of steep topography and limited street lighting. Many streets within the LCA only have a footpath down one side, and it is common for streets to not feature any footpaths. Some areas of footpaths are in poor condition and may present a hazard to those moving through the LCA. Grass verges are common and, despite often being narrow, support a large number of street trees which are a mix of mature and more recently planted native species which should grow into good quality street trees. Some narrow streets may only feature tree planting on one side, which is generally on the opposite side to the footpath leaving pedestrians unprotected from the elements.



The residential scale of the LCA is generally characterised by one and two storey built forms. However the topography has a substantial influence on the relationship of the building with the streetscape. This is most prevalent on sites located to the east of the LCA where buildings can feature a taller built form, especially when garages are located in front of the building, on the lot line.



The character area features predominantly interwar single-story detached residential dwellings with brick facades, hipped and gable roofs which forms a clearly identifiable character and style.



Housing features consistently aligned front setbacks and landscaped front yards with low and open front fencing. Driveways are normally single crossovers which are located to the side of the property, creating an open well-spaced pattern of development.

5.6 FUTURE CHARACTER

DESIRED FUTURE CHARACTER

The South Earlwood Local character area should maintain its existing character as a low density residential area. The primary development type should be low Scale detached housing within a landscaped setting. Care should be taken when allowing alternate residential buildings to ensure they are of a form and Scale consistent with the existing area. Higher density development should be guided by the controls within Councils LEP and DCP. This will ensure that any future development is compatible with the existing character of the area.

The maximum residential building height should be kept to two storeys however this may not be possible on sites with challenging topography. Development on sloping sites should be designed to step down or across the landform to lower bulk and Scale. The pattern of side setbacks will be retained to ensure separation between houses. Front setbacks should be generous and consistent with the prevailing street setback. Yards should feature soft landscaping and native tree planting should be encouraged to improve microclimate and tree coverage through the area. Front fencing will be low and open to reinforce the open nature of the streets.

To ensure they don't become a dominant urban feature, driveways should be spaced to retain on-street parking and maximise areas for grassed verges and street tree planting. Driveways should be single crossovers to minimise hard surfaces in front setbacks and maximum space soft landscaping in the private domain. Garages are to be located behind the front building line on flat sites, and set back from the front boundary to accommodate a parked vehicle. Where this is not possible, such as on sites with challenging landforms, garaging should ideally be located avoid it becoming a dominant façade feature.

New buildings should not feature large unbroken areas of monotonous materials, and should incorporate feature to reduce apparent bulk and Scale such as modulated façades and shade elements. Colours and materials should be selected to minimise the urban heat island effect, and to harmonise with the existing character of the area.

Streets should continue to be well maintained however they will benefit from provision of additional streetlighting. Council has the opportunity to improve the level of walkability by enhancing the quality of the existing footpaths and extending the existing pedestrian network with new sections of footpath.

5.6 FUTURE CHARACTER



Future residential developments should incorporate modulated elements such as rendered entry porticos. The garage should be located behind the front building line so it does not dominate the façade, and driveways should be single crossovers.



The use of different materials and finishes on the residences such as on different storeys or areas of wall is encouraged as it reduces apparent building bulk which is important to retain the low scale character of the LCA.



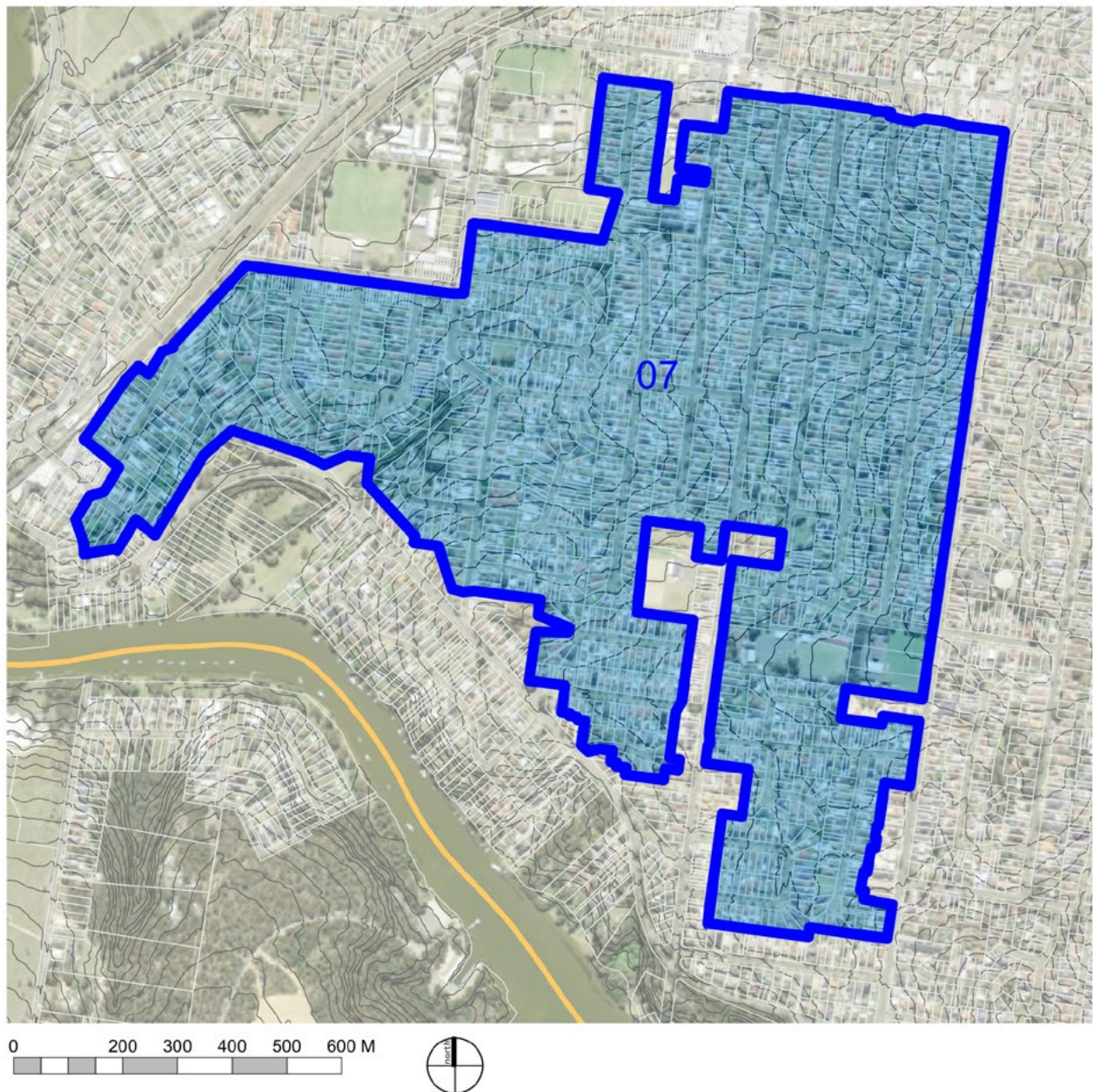
This example of a recently completed residence within the LCA features a brick façade, tiled hip roof, low fencing and a landscaped front setback reinforces the existing character of the building and the open streetscape.

5.6 RECOMMENDED PROVISIONS

OBJECTIVES AND CONTROLS

Objective		Recommended Controls
O1 Maintain the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
	C2	Minimum lot size is to be 450 m ²
	C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
	C4	Minimum front setback is to be 5.5m
	C5	Maintain residential side setbacks consistent with the established street pattern.
	C6	Minimum side setback to be 0.9m clear of obstructions
	C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
	C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
	C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
	C10	Maximum height of front fence to be 1.2m
	C11	Provide street trees of an appropriate native species and increased in numbers.
O2 Maintain the low scale of the LCA.	C12	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O3 Maintain the articulated built form which characterises the area.	C13	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
	C14	Provide a medium pitched primary roof form consistent with the area.
O4 Maintain a cohesive palette of materials and colours.	C15	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
	C16	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
	C17	Provide materials and colours that are light or mid-tone and complementarity with the existing finishes in the area.
O5 Minimise the visual impact of off street parking on the streetscape.	C18	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision
	C19	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
	C20	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

5.7 LOCAL CHARACTER STATEMENT



EAST HILLS AND SOUTH-WEST PANANIA

This Local Character Area is located in the south-west of the Canterbury Bankstown LGA. The LCA encompasses areas from multiple suburbs including East Hills, the south-west of Panania, and a small section of Picnic Point. The area is bound to the west by the T8 Trainline which curves around the LCA, and Picnic Point Road is the eastern boundary. The Southern boundary is formed by the residential lots facing onto the north of Henry Lawson Drive and Doris Street where there is a marked change in the topography. The stepped northern boundary is formed by the lots facing onto Tower Street and the educational institutions north of Singleton Avenue and Lehn Road.

The LCA generally fall from the higher points in north-east of the Area the towards the western and southern boundaries of the LCA. The fall is not uniform across the LCA and there are sections of rolling topography alongside flatter areas. Given the relatively gentle slope in the area the changes in the topography are most apparent on the longer streets where the rise and fall can be seen. The road network in the east of the LCA has a mostly axial structure where the roads primarily run in a north-south orientation create rectilinear, and often very long, residential blocks. The west of the LCA features a

5.7 EXISTING CHARACTER

number of constraints resulting in the formation of an organic road network and irregular and often deep block shapes. There are limited views to the treeline to south of the area, however there are no real significant views possible in the area.

The residential lots within the Character Area feature a broad range of sizes and, while the most are rectilinear in form, also feature a diverse range of lot shapes. A characteristic of some blocks is the changeable position of rear boundaries where changes in lot depth can lead to large variation in lot sizes on lots with consistent frontage widths. The majority of lots have an area of 500 – 600 sqm, and significant proportion range 600 – 700 sqm in area with both size ranges featuring a mix of rectilinear and irregular shapes, and lots smaller than 500 sqm are located throughout the LCA. Lots of 700 - 900 sqm are not uncommon especially around the centre of the LCA, these are normally rectilinear deep lots that generally retain similar widths but feature larger depth than lots with smaller sizes. The number of lots with sizes of 250 – 300 are increasing across the LCA as larger lots are subdivided following being redeveloped as attached dual occupancies. The smallest lots of 220sqm are located to the South of Lehn road, however these have been developed with single residences built across two lots, and give them the impression of being larger single blocks.

Lots have width ranging between 12.5 – 15.5m or the most common in the LCA. However there are a number of wider lots featuring widths of 16-20m which are irregularly distributed throughout the LCA. The lot widths feature in many streets are irregular, with variations are in part due the road structure, but most often when individual or small grouping of wider lots alter the pattern of lot widths within a block. There are a large number of lots less than 12.5m including the lots facing onto Kelvin Parade, in groups towards the centre of the LCA where they are often located between wider lots on the long north south oriented streets. Lot widths of 7.5-9m are generally found on lots that have been subdivided to accommodate dual occupancies. The narrowest lots of 6m face Lehn Road have been developed as single detached dwellings built across two lots presenting as a wider wide lots to the street.

Residential front setbacks normally range between 6-8m and sites developed which are consistent between both detached residential and attached dual occupancies. Larger setbacks of 9 – 13m are generally only seen on the deepest lots featuring older housing, or on the irregular shaped lots. The smallest setbacks of 5m are normally restricted to housing on the short ends of blocks where lot depth is shallow.

The LCA is characterised as a low density residential area where the predominant building typology is one and two storey detached residential dwellings on single lots. Additional residential building types include an increasing intensity of attached dual occupancies and a small number multi-residential development. The oldest housing in the LCA has been constructed post WWII and can be found throughout the LCA however are most common in the north and west of the area. They single storey detached housing featuring wide side setbacks, feature tiled pitched roofs, and are commonly finished with light-coloured weatherboard or fibro cladding, with some constructed with red face brick.

The majority of housing in the area has been constructed post the 1970's. These detached dwellings are located across the LCA and can feature one or two storey built forms under a pitched tiled roof. They are most commonly finished with face brick are fully rendered in a range of restrained tones. The newest detached residential housing in the area, constructed from the 21st predominantly have a two-storey building height, with roofing mix of traditional pitched and some non-traditional forms. These are likely to feature multiple materials on a façade most commonly a combination of masonry brick and render. Dual occupancy developments are generally two storeys and commonly feature non-traditional roof forms, alongside extensive parapets, boxed balconies and the occasional application of high-gloss materials which can combine to add to height and bulk of the built form. The newer detached residential developments are generally compatible with the overall character of the LCA. However a number of developments, most commonly attached dual occupancies, stand in sharp contrast with the existing character due to a combination of their scale, design and materiality.

The majority of properties do not have front fences the front boundaries, and grass verges often continue seamlessly into the front setbacks. Where older properties feature front fencing it is normally low brick walling or open fencing in the same finishes and style of the corresponding residence. Newer residential

5.7 EXISTING CHARACTER

developments are most likely to feature front fencing. These often have a more substantial appearance than older examples, however they are generally consistent with the scale and character of the overall approach in LCA. Front yards generally feature extensive lawns, and where gardens are present the most substantial planting tends to be located close to the building line or adjacent the side boundaries. As the public/private domain interface is often unbroken many streets have a wide and open feel.

The oldest housing in the LCA have driveways located adjacent the side boundary which generally continue to the rear of the property, with off street parking normally in under lightweight open carports positioned to the side of the house, with some located forward of the building line on the narrowest lots. Developments from the latter half of the 20th century forwards have driveways stopping at the building line. These connect to single or double garages, with the widest garages most common on the newest detached developments. Detached dual occupancy developments feature single garages either located adjacent the side boundaries with separated single width crossovers, or in the centre of the subdivided lots connecting to double width crossovers and a higher ratio of hard surfaces forward of the building line than seen throughout the LCA. While the majority of properties feature setbacks that can accommodate parking forward of the building line, on-street parking of additional vehicles is common especially in areas with larger numbers of higher intensity development. This can reduce the existing amenity where the narrower streets can become congested, and the additional of new driveways crossovers reduce the space available for vehicle parking whilst increasing the overall.

The LCA features a number of smaller reserves and parks which can be accessed by residents throughout the area, and are supplemented by the extensive open space on the banks of the Georges Rive directly to the south of the Area. Phillip Park, Duke Reserve, and Matthews Reserve feature play equipment alongside grassed areas at a range of sizes and limited tree planting. While Eileen Reserve, Carlowrie Reserve and Malvern Reserve provide open space surrounded by some light tree planting. The Picnic Point Bowling & Social Club hosts casual and competitive lawn bowls and also provides the LCA with entertainment and dining options. The large Smith Park and the George Jacobsen Tennis Centre are located adjacent to the north-west boundary and host a wide array of casual and competitive sport.

The area is primarily residential with most commercial activity in the form of home businesses. However there are some 'corner shops', specialist retail and commercial frontages and a smash repairer within the LCA. Residents are able to access the East Hills Local Centre to the West of the LCA, and the Larger Panania Local Centre to the north of the Area. These which both provide a range of retail, business dining and entertainment to the services to the LCA. The area is very well serviced with schools and a range of community facilities within or in close proximity to the LCA. There is a small number of childcare centres located within the Area and the St Christopher's Catholic Primary School and the Panania Public School are located within the LCA. And residents are able to easily access the The East Hills Primary, East Hill Boys Secondary and East Hills Girls Secondary Schools which are located adjacent to the north-eastern east boundary of the LCA. The Panania Anglican Church and the East Hills Baptist Church provide religious services within the area. The Panania Library and Knowledge Centre is located in close proximity to the north of the LCA provides a range of community activities and services.

Henry Lawson Drive at to the south of the LCA which provides vehicular access to the adjacent suburbs east and west of the Area before connecting into the wider Sydney metropolitan road network. Tower Street at the north of the LCA provides local connections which provide access to Panania north of the trainline and to the suburb of Revesby, east of the Area. The long streets in the east of the LCA facilitate easy vehicle movement from the north to the south of the LCA. However east to west movement across the area can be somewhat constrained especially in the west of the LCA where streets tend to be narrow and there is lack of permeability within the more organic road network.

The East Hills Train station services the west of the LCA which connects to the T8 Trainline, and residents in the north of the area are able to use active transport methods to reach Panania Station. However given the size of the area most residents, especially those in the south east, will need to use public bus services or vehicles access these stations. Bus services loop through the area at semi-regular intervals providing connection throughout the day and evening to the Revesby Local centre and Central Bankstown, as well as the train stations and Smaller Centres of East Hills and Panania adjacent the area.

5.7 EXISTING CHARACTER

The higher trafficked roads in the LCA have footpaths to both sides of the street. However secondary streets generally only have footpaths to one side and the footpaths across the LCA area in mixed in quality and condition. Some streets do not feature footpaths however this is generally restricted to the streets terminating in cul-de-sacs where grass verges continue from the curbing to the front boundary. The length of some blocks, especially in the east of the Area, can create long walking distances and increase the travel time required to move between some areas within the LCA. This is mitigated in some instances where pedestrian links are located between cul-de-sacs and adjacent points of interest, however these are not a common feature of the LCA.

Street trees are a mix of mid-size native species which have provide a fair degree of shade protection for pedestrians. Street trees are often planted regularly to both sides of the street. Some of the secondary streets only feature planting to one side and some streets are notable for having large gaps between tree coverage, or a lack meaningful street tree planting, although this is uncommon within the LCA. Street lights are located at regular intervals on the primary and secondary streets providing good a high degree of coverage throughout the LCA.



Most streets feature detached housing from a range different eras. Common elements between developments such as roof form, materiality and scale reinforce the character of the LCA.



Many houses do not feature fencing and have large lawns forward of the building line. This creates an open feel on many streets.



Many streets feature regular street planting of medium height native species. This provides extensive shade coverage of pedestrian environment and helps to reduce the heat island effect.



Some residential developments have built form outcomes that are not compatible with the desired character of the LCA. Recessive entries, prominent garages, and elements such as boxed balconies can create bulky façades.

The quality of the streetscape can be reduced where developments have no window openings facing onto the street at ground level, excessive hardscape and little landscaping in the front setback.

5.7 FUTURE CHARACTER

DESIRED FUTURE CHARACTER

The East Hills and South-West Panania Local Character Area has undergone a period of change in recent years due to the substantial amount of increasingly diverse residential redevelopment which has occurred throughout the LCA. These changes to the residential built form and fabric of the public realm have had a mixed impact within the LCA. This is most apparent where intensity or form of development that is incompatible with the existing character has reduced, rather than improved or increased, the overall quality and amenity of the area.

Future development within the area should enhance the existing low density residential character of area. The primary development type within the LCA should be low scale detached residential housing within a landscaped setting. Care needs to be taken when allowing higher density residential building types. These developments may impact the existing amenity or not reflect the existing character of the area. Where permitted these developments should be guided by the controls set out in Councils LEP and DCP. This will ensure the future built form outcomes are compatible with, and will enhance the existing character of the area.

To ensure the low scale character of the area is maintained the maximum residential building height should be two storeys and allow for traditional medium-pitched roof forms. The existing pattern of front setbacks in the area is generous, which allows for large front yards. Future residential buildings should align with the prevailing setback pattern on the street to reinforce this characteristic of the area and provide the opportunity for landscape in the front setback.

New residential developments should feature modulated façades, incorporate shade elements, and feature habitable rooms with windows facing onto the street at ground level as this will reduce apparent bulk in the building form. They should avoid extreme roof pitches, high gloss finishes and large areas of monotonous surface treatment. Colours and materials should be selected to minimise the urban heat island effect, and to complement the existing character of the area.

Where front fencing is used to delineate front boundaries it should have a low height in order to maintain the open feel between the public and private domain. Front yards should feature large areas of soft landscaping, and native tree planting should be encouraged to improve microclimate and enhance the existing tree coverage in the area.

New driveways should be single crossovers to minimise the prominence of the driveway and hard surfaces in the street and within front setbacks. Where multiple driveways are proposed they should be located to retain on-street parking which will maximise the remaining for grassed verges and street tree planting. Garages are to be located behind the front building line and set back from the front boundary to accommodate a parked vehicle fully within the lot boundary.

The existing vehicle network should be maintained. Opportunities to enhance the quality of the streetscape by increasing the extent and quality of footpaths coverage, and infilling gaps in street tree coverage should be pursued by council.

5.7 FUTURE CHARACTER



Where residences feature larger built forms the use of different materials finishes on the residences such on different storeys or areas of wall is encouraged. This will help to reduce the apparent building bulk which important to ensure new development retains the low scale character of the LCA.



The use of a good mix of materials and colours, deep eaves, a prominent entry, and a first floor setback behind the building will break down the bulk of a built form and create interest in the facade.

The setback garage, narrow driveway, open front boundary, and large areas of soft landscaping which should mature with dense shrub coverage will enhance the quality of the streetscape.



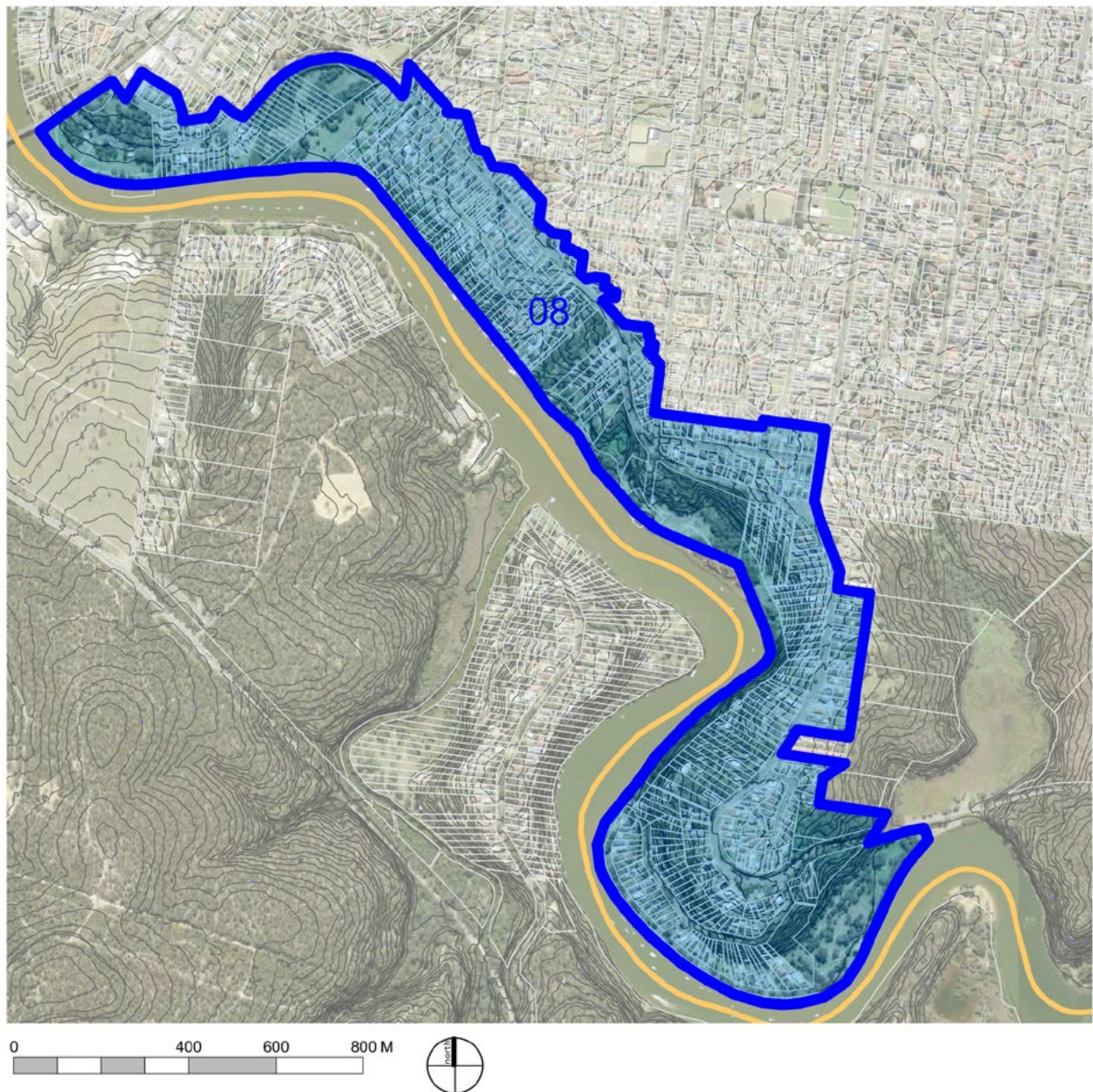
Future residential development should be at a height consistent with desired low scale character of the area. Pedestrian entries should be located forward of the garage, front fencing should be low or open, and front setbacks should feature soft landscaping.

5.7 RECOMMENDED PROVISIONS

OBJECTIVES AND CONTROLS

Objective		Recommended Controls
O1 Enhance the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
	C2	Minimum lot size is to be 450 m ²
	C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
	C4	Minimum front setback is to be 5.5m
	C5	Maintain residential side setbacks consistent with the established street pattern.
	C6	Minimum side setback to be 0.9m clear of obstructions
O2 Enhance the public and private domain interface	C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
	C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
	C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
	C10	Maximum height of front fence to be 1.2m
	C11	Provide street trees of an appropriate native species and increased in numbers.
	C12	Provide footpaths to enhance pedestrian amenity
O3 Maintain the low scale of the LCA.	C13	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O4 Maintain the articulated built form which characterises the area.	C14	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
	C15	Provide a medium pitched primary roof form consistent with the area.
O5 Enhance the range of materials and colours.	C16	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
	C17	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
	C18	Provide materials and colours that are light or mid-tone and complementary with the existing finishes in the area.
O6 Minimise the visual impact of off street parking on the streetscape.	C19	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision
	C20	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
	C21	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

5.8 LOCAL CHARACTER STATEMENT



EAST HILLS SOUTH AND SOUTH PICNIC POINT

This Local Character Area is located in the south-west of the Canterbury Bankstown LGA. The LCA is adjacent to the Georges River and it includes the south of East Hills – East of the T8 Trainline, and the south Picnic Point. The Area is bound to the west by the East Hills Local Centre and the T8 Trainline, and to the east by the Georges River National Park. The northern boundary is formed by the lots on the northern side of Henry Lawson Drive and Doris street. The southern boundary is formed by the Georges River which snakes around the length of the LCA.

The highest point within the LCA is adjacent to the eastern boundary where the ridgeline extending into the LCA from the north forms a peak adjacent to Sylvan Grove before descending down to the Georges River. The changeable topography is more pronounced in the centre and east of the LCA where the areas of steeply sloping topography influence the road structure, lot width and depth, as well the height and built form of the residences on some sites. Where the land flattens out adjacent to the areas have been utilised for a diverse range of public reserves. Views of the trees that line the Georges River can be seen throughout the LCA. Significant regional views can be seen from throughout the area, but are most

5.8 EXISTING CHARACTER

common in the east and centre of the LCA where the raised elevations allows panoramic views across the river to the Georges River National Park located to the South of the LCA.

The roads and streets within the LCA generally follow the natural features of the LCA. Henry Lawson Drive, the primary road in the area, follows the alignment of the Georges River, the secondary streets are generally narrow, sloping and tend to be oriented in response to the landfall. The changing topography and organic road structure has resulted in ample opportunities for views across the LCA to the Georges River and the Parks which line its banks, and to the National Park located outside of the of the western bank of the Georges River.

The majority of lots are located on the sloping land between Henry Lawson Drive and the Georges River. As a result, the size and shape of the residential lots vary dramatically across the area however this is generally not perceptible from the public domain as the variations are often to the rear of properties and are obscured by the built form. Whilst there are some streets which feature lots with regular lot forms many lots, especially those in the east of the LCA, have an irregular or skewed form which is often in response to the organic road network. The LCA features a large number of battle-axe lots which are often set down from the street at the end of long access handles.

Residential lot sizes are characteristically very deep, and as a result they commonly have large footprints of between 900sqm to 1250sqm with some larger lots exceeding that size. Lots between 600 and 900 sqm in size are common, however many are the result of subdivision of large lots and have a battle-axe form. There are a small number of 500 – 600 sqm lots which tend to have a smaller regular block form and are generally located in the centre of the LCA. The smallest lots of approximately 300sqm are the result of subdivision of larger blocks to accommodate attached occupancy developments, most commonly in the centre of the LCA.

Lot widths of between 15-16m are the most common within the LCA and there are a significant number of wider lots with widths of 18-20m, and lot widths exceeding 20m are not uncommon in the east of the LCA. There are a limited number of smaller lots with 12 – 14m, and the smallest lots are the result of subdivision to accommodate dual occupancy development and are consistently 7.5m wide. Whilst some streets within the LCA feature a consistent pattern of lot width, it is more common for lot width to be highly variable across the length of a street, particularly on curved roads.

Font setbacks across the LCA are generous, being between 9 – 13m. However front setbacks greater than 20m to the residential building are common on the deep lots the south-western side of Burbank Avenue. Lots on Henry Lawson Drive can feature verge large setbacks of over 30m, however in many instances these properties feature detached garage or carport structures located forward of the primary residence which align more closely with the prevailing setback in the LCA.

The majority of residential buildings within the LCA are one or two storey detached residences on single lots. There are some examples of 3 storey built forms, although these are generally limited to the steeply sloping lots north of Carinya Avenue or lots with rear boundaries onto the Georges River, although these often present a one or two storey frontage to the street. There are an increasing number of two storey attached dual occupancies being developed in the area. Whilst this building type appear across the LCA most examples are concentrated in a group on Burbank Avenue, and a small cluster in the east of the LCA along Henry Lawson drive.

The housing within the area has been developed over a number of years and presents a vast range of architectural styles and materiality. The majority of older detached residences in the LCA feature pitched roofs with tiled lining and finished with faces brick, or limited instances of render. The newest housing in the LCA are often finished with large areas light render and may incorporate additional materials such as brick or natural stone, steep or non-traditional roof forms. Detached dual occupancies are likely to feature extensive parapets, boxed elements and non-traditional flat roofs which can add to the apparent height

5.8 EXISTING CHARACTER

and bulk of the built form. Whilst some new residential developments feature a desirable built form, scale and materiality outcome, many recent developments have continued the heterogenous character of the LCA.

The height and form of walls and fences on front boundaries within the LCA is mixed. The secondary streets such as Burbank Avenue, Doris Street, and the majority of Carinya Road generally do not feature front fencing or feature low walling, with small sliding gates becoming more common on newly constructed fencing. Residences facing onto Henry Lawson Drive are more likely to feature walling on the front boundary with some examples notable due to their height and substantial appearance which contrasts with the prevailing approach in the area. Some sites in the east and west of the LCA feature retaining walls in response to the sloping topography on these sites. Landscaped front gardens are common throughout the area with many featuring extensive planting, and wider lots often feature areas of lawn within the front setback.

A large number of lots have been developed with double garages and many have areas of hardscape forward of the building line to accommodate multiple vehicles, with some lots featuring extensive driveways to account for the slope. The newly developed dual occupancies often feature a double crossover with limited examples of separated single crossovers. On the wider and deeper lots in the centre and east of the LCA, detached garages and carports are often located forward of the residential building line with some oriented so parking is perpendicular to the front boundary, which require wide areas of driveway to allow for turning circles. The incorporation of large area of hard surfaces within front setbacks is common throughout the LCA. This has led to some areas appearing to be dominated by driveways and vehicles. Despite the prevalence of off-street parking provision, the streets are extensively used for parking. While Henry Lawson drive is wide enough to accommodate parking, this becomes problematic on the narrow streets where the limited space can easily become congested.

The LCA is notable for its large number of public parks and reserves which are located across the length of the Area. These vary in size and character however most located on the banks of the Georges River with the exception being the Sylvan Grove Native Garden which is adjacent to the ridgeline on the eastern boundary of the LCA. East Hills Park which features the Caird's Wharf heritage item and Monash Reserve are the largest parks in the west of the LCA which provide space and facilities, supporting a wide range of outdoor activities. Lambeth Reserve is located in the centre of LCA which forms the western end of a network of interconnected spaces including the Picnic Point Reserve, the Alan Ashton Reserve and Fitzpatrick Park in the east which links into the Georges River National Park beyond the boundary of the LCA.

The Area is primarily residential and commercial activity generally takes the form of home businesses. Residents need to travel out of the area for most services and shopping. The East Hills Local Centre to the west of the LCA provides a small range of retail, business and hospitality services however those in the east of the LCA require a vehicle to travel to the Centre. The Area does not include any formal educational facilities or childcare centres, however a number of schools are located in the adjacent suburbs to the north of the LCA. The Picnic Point Scout Group is located in the east of the LCA however the areas lacks indoor community facilities.

Vehicular Connectivity across the LCA is generally very good. This is due in large part to the limited number of streets which are either connected directly to or a short travel distance from Henry Lawson Drive which runs the length of the LCA. This provides regional access to the greater Sydney metropolitan road network. Access to public transport within the LCA is mixed across the Area. Residences in the west are able to easily access the T8 Trainline through East Hills Train Station. Residents in the centre LCA are able to utilise the regular bus services which provide connection to Panania and the East Hills Local Centres throughout the day. Residents located in the east of the LCA are able to access Panania and Revesby via bus service. However services are limited to a small number of departures during mid-morning and mid-afternoon.

5.8 EXISTING CHARACTER

The quality of the pedestrian environment in the LCA is mixed due to a range of factors including the extent and quality of footpaths, the often steep topography of the area, and the limited amount of street trees. Cook Crescent and Henry Lawson Drive are the only streets within the LCA to feature footpaths. The provision of footpaths within the LCA is limited to one side of Cook Crescent and Henry Lawson Drive which has sections where footpaths are on both sides of the road, down one side and lengths where there are no footpaths and a poor-quality edge condition. The secondary streets not feature footpaths and grass verges are often narrow and can be easily obstructed by objects such as parked vehicles which can force individuals to enter the road to move through an area. There is a good degree of street lighting in the LCA however street tree coverage is poor. The limited number of street trees are often tall native species with open canopies that do not provide shade coverage to the pedestrian environment.



Front setbacks on secondary streets are commonly open, allowing extensive areas of garden to enhance the streetscape. Housing is often located on sloped land. This can allow for regional views across the rooftops where housing is low scale and is located on the low side of the street.

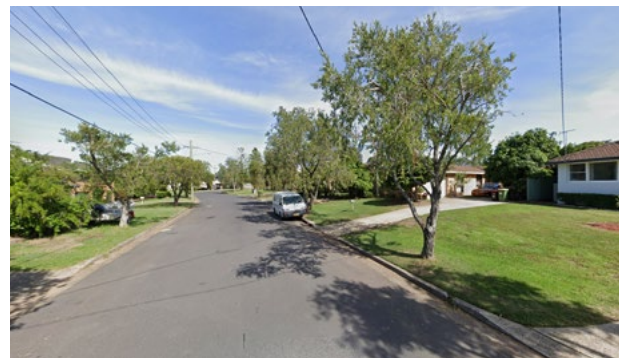


The area features an increasingly diverse range of residential built forms. Many houses within the LCA are set on deep lots with large setbacks, these can often feature large areas of hardscape forward of the building line rather than soft landscaping. This can reinforce the primacy of the vehicle in the street, particularly in areas lacking street tree coverage.



New dual occupancy developments within the LCA often feature extensive areas of hardscape in the front yard. This has reduced the space for soft landscaping and contributed to the primacy of the garage in the street.

In some instances the additional driveways on to the street have reduced the pedestrian amenity where the existing street edge has been eroded down by additional vehicle crossings



Some streets within the LCA feature a higher concentration of older housing. These areas often feature a higher quality pedestrian environment due to the number and quality of street trees wide verges and open public/private domain interface.

5.8 FUTURE CHARACTER

DESIRED FUTURE CHARACTER

The East Hills South and South Picnic Point Local Character Area features a diverse range of housing types, built forms and street addresses. This has created a heterogenous character in some areas within the LCA with much of the recent residential development having a mixed impact on the overall quality of the public realm. This is most apparent where the intensity or form of development is incompatible with the desired character, and where opportunities to improve the overall quality and amenity of the area have not been realised.

Future development within the area should enhance the existing low density residential character of area. The primary development type within the LCA should be low scale detached residential housing within a landscaped setting. Care needs to be taken when allowing higher density residential building types. Especially noting the potential for these developments to impact the existing amenity or otherwise not reflect the existing character of the area. Where permitted these developments should be guided by the controls set out in Councils LEP and DCP. This will ensure the future built form outcomes are compatible with, and will enhance the existing character of the area.

To ensure the low scale character of the area is maintained building height should be kept to two storeys and allow for traditional sloped roof forms. This may not be possible on sites with challenging topography, however building should be designed to step down or across sloping landforms in order to lower bulk and scale. This will help to ensure that future residential development located on the sloped banks of the Georges River, or on the tree lined slopes east of the LCA, do not become an intrusive visual element in the landscape.

The pattern of front setbacks across the LCA are generous, with large setbacks on both flat and sloping sites. Future residential buildings should align with the prevailing setback pattern and it is strongly encouraged to utilise the larger areas forwards the building for the provision of soft landscaping including large shrubs and canopy trees.

To reduce apparent bulk in the building form new residential developments should feature modulated façades, incorporate shade elements, and incorporate multiple materials within the façade treatment. New residential developments should not feature extreme roof pitches, excessively bulky façade elements, the use of high gloss finishes or large unbroken areas of monotonous materials. Colours and materials should be selected to minimise the urban heat island effect and to enhance the existing character of the area.

New driveways should be single width, and excessive areas of hardscape in the front setback should not feature in new developments. Driveways are to be light in colour to reflect heat and minimize the heat island effect. Where front fencing is used to delineate front boundaries it should have a low height and an open form. Front yards should feature large areas of soft landscaping, and native tree planting should be encouraged. This will help to minimise the prominence of driveways and hard surfaces in the street, and enhance the public domain.

Where multiple driveways are proposed, such as on dual occupancy developments, they should be located to retain on-street parking which will maximise the remaining for grassed verges and street tree planting. Garages are to be located behind the front building line and set back from the front boundary to accommodate a parked vehicle fully within the lot boundary. Where this is not possible, such as on sites with challenging landforms, garaging should be located to avoid it becoming a dominant feature on the façade or within the front setback.

The existing vehicle network should be maintained however much of the pedestrian environment is poor. Extending the coverage and improving the quality of footpaths, provide new landscaped verges, enhancing the existing verges, and increase street tree planting will greatly improve the street edge condition and amenity for residents.

5.8 FUTURE CHARACTER



Future residential developments should incorporate modulated façades, windows from habitable rooms on the ground floor, and soft landscaping forward of the building line. The use of different materials and finishes within the building fabric is to be encouraged as it can help to break down bulk and create interest in the façade.



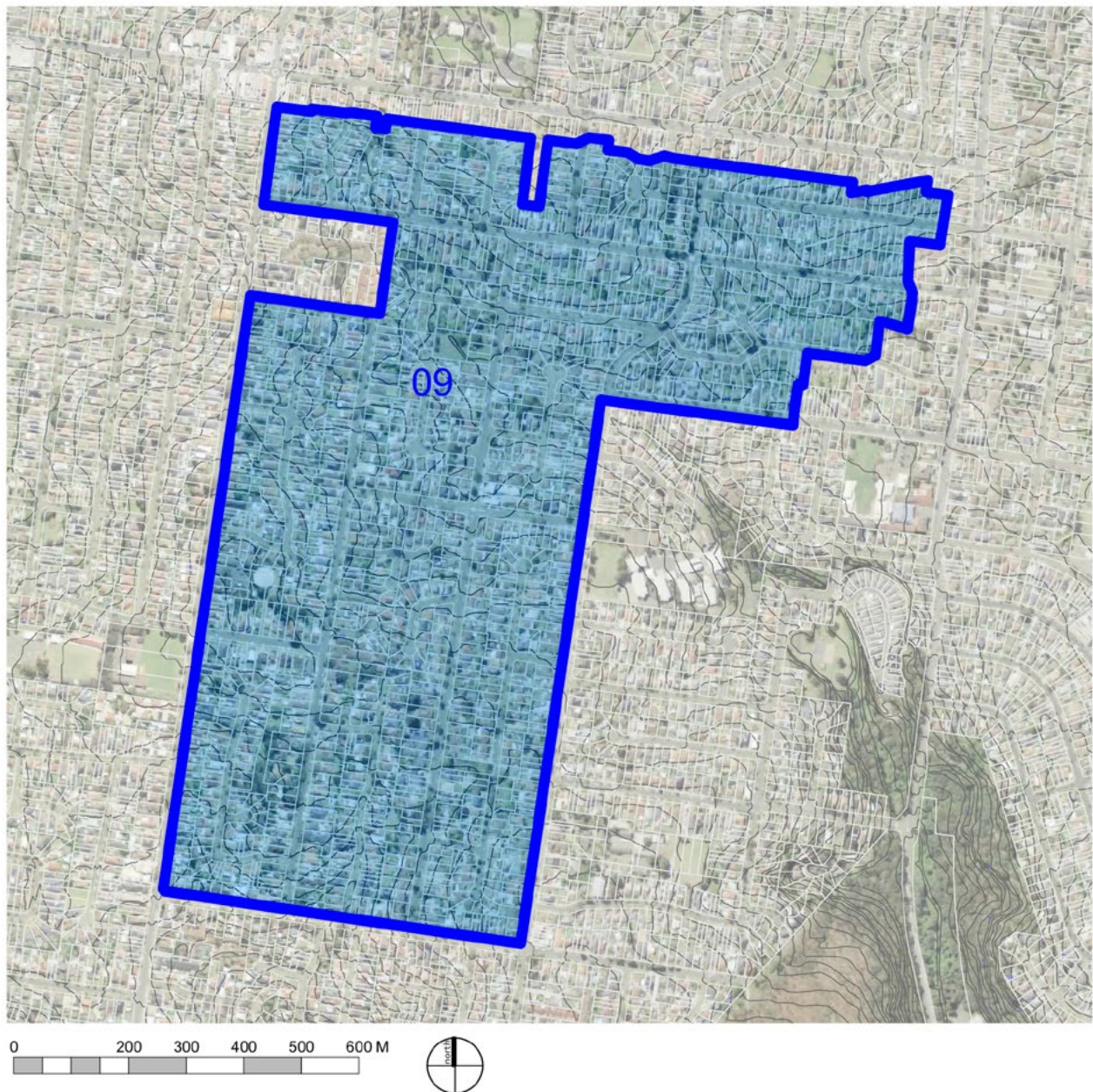
Future residential development should be at a height consistent with desired low scale character of the area. Pedestrian entries should be located forward of the garage, front fencing should be low or open in front of an area of soft landscaping.

5.8 RECOMMENDED PROVISIONS

OBJECTIVES AND CONTROLS

Objective		Recommended Controls
O1 Enhance the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
	C2	Minimum lot size is to be 450 m ²
	C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
	C4	Minimum front setback is to be 5.5m
	C5	Maintain residential side setbacks consistent with the established street pattern.
	C6	Minimum side setback to be 0.9m clear of obstructions
O2 Enhance the public and private domain interface	C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
	C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
	C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
	C10	Maximum height of front fence to be 1.2m
	C11	Provide street trees of an appropriate native species and increased in numbers.
	C12	Provide footpaths to enhance pedestrian amenity
O3 Maintain the low scale of the LCA.	C13	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O4 Maintain the articulated built form which characterises the area.	C14	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
	C15	Provide a medium pitched primary roof form consistent with the area.
O5 Enhance the range of materials and colours.	C16	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
	C17	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
	C18	Provide materials and colours that are light or mid-tone and complementary with the existing finishes in the area.
O6 Minimise the visual impact of off street parking on the streetscape.	C19	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision
	C20	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
	C21	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

5.9 LOCAL CHARACTER STATEMENT



SOUTH-EAST PANANIA AND WEST PICNIC POINT

The Local Character Area is located in the south west of the of the Canterbury Bankstown LGA. The area encompasses the south-east corner of Panania, the north of Picnic Point, and captures a small section of the suburb of Revesby. The northern boundary is formed by the lot facing onto Tower Street and the eastern boundary is formed by Picnic Point Road. The irregular shaped area has two southern boundaries of Ferndale Road and Burns Road which are linked by Kennedy Street which also forms most of the eastern boundary. These streets mark the transition between the suburban residential setting within the LCA with the sloped bushland setting of the areas to east and south of the LCA. The northern section of the eastern boundary is setback from The River Road and follows the extent of a high intensity area of multi-residential and dual occupancy development which contrasts sharply with the low-density character of the LCA.

There are some high and low areas within the LCA however the transition between these points often gradual. Changes to the topography are most apparently on the longest streets where the rise and fall of the land can be seen across long site lines, however the high points do not provide any significant views

5.9 EXISTING CHARACTER

of the wider context. The area features a rectilinear road network where the majority of the roads have a north-south axis and run continuously through the length of the LCA. These have formed a series of wide blocks that are often broken down by a network of narrow, often curved streets terminating in cul-de-sacs.

The majority of lots feature a deep rectilinear form oriented to the axial road network. Lots with irregular forms are generally on the secondary streets within the wider blocks, and feature irregular frontages or skewed side boundaries in response to the organic curves which characterise many of these streets. Despite the variance in shape lot widths at the building line consistently fall between 15-16m. Lots with smaller widths ranging between 12-13m are most common in the northeast and north-west of the LCA. There are a large number of wide lots of 18-20m within the LCA, however these are most often located in the south of the LCA where they face onto Ramsay Road, Victor Avenue and Kennedy street, south of Paul Street. The smallest lots with the LCA range between 7.5-10m however these are generally restricted to sites featuring attached dual occupancy development.

The area feature a wide range of lot sizes, with variation often resulting from a combination of changes to depth, width and shape. The majority of lots within the LCA have sizes of 600 - 800sqm and face onto the primary axial roads in the area. Lots of 500 – 600sqm are most often located towards the north-east of the LCA or on the secondary streets within the longer blocks in the south of the Area. The largest lots of 850 – 1000 sqm are often on the widest lots in the LCA are most common the south of the LCA. The smallest lots 250-450sqm are generally only seen on the Dual Occupancy development in the area. Despite the variability in lot size, front setbacks of 6.5-8m are consistent throughout the LCA. Variation is generally limited to a few examples of older housing which larger setbacks of up to 10m, or some newer developments featuring slightly smaller setbacks of around 5.5m.

The area is characterised as a low density residential area and the primary typology is generally either one or two storey detached housing on single lots. However there has been a substantial amount of redevelopment of increasingly diverse residential development which has expanded to include two storey attached dual occupancies which are becoming a common typology within the area. The area features a small number of low Scale multi-residential developments with two storey heights, comply with the prevailing front setbacks, and feature masonry construction and pitched roof forms.

The older housing in the area is generally single storey, clad in lightweight weatherboard, and feature a tiled pitched roof. The majority residential developments in the area are from the late 20th century are generally finished with masonry brick or render and feature pitched roof forms. The newest residences form the 21st century onwards are most likely to have a two storey built form, incorporate a traditional pitched roof, and feature the use of multiple materials within the façade. Dual occupancy developments are most likely to feature non-traditional roof forms, extensive parapets and other elements such as high gloss cladding, and boxed balconies on the front façade. The majority of the new detached residential dwellings are consistent with the character of the area. However the bulk, Scale and materiality of some built form outcomes, most commonly on dual occupancy developments, are not compatible with the desired character of the LCA.

Front fencing is consistently either low masonry or open fencing, however many properties feature open front boundaries and allow the grass verges to continue into the front setbacks. The majority of lots feature lawns forward of the building line, although these are less common on narrow sites featuring dual occupancies, where the driveways often limit the space available for soft landscaping. Front gardens are common throughout the LCA, however the substantial and mature planting is most likely to be seen on lots with older housing.

Off-street parking provision in the area is mixed and often reflects the age of development. The oldest housing within the area generally feature single width, open carports located to the side of the property although in some instances parking may be an uncovered driveway or area of hardscape. Later developments generally feature garaging within the building envelope. These are often single garages with additional off street on the single width driveways, or occasionally on additional areas of hardscape located forward of the building line. The newest detached residences are most likely to feature

5.9 EXISTING CHARACTER

double garages with corresponding wide driveways and street crossovers. Attached dual occupancy developments generally feature single garages connected to separated single driveways when located on opposite sides of the development, or double driveways when garages are co-located in the centre. Many of the secondary streets are used for parking of private vehicles. This often creates congestions on the more narrow streets and can be problematic where there existing on street parking provision is reduced follow the construction of additional driveway crossovers. This is common in streets with higher intensity of dual occupancy development.

While the LCA does not feature formal sports facilities, the area contains a large number of parks and reserves. These support a range of outdoor activities, and are easily accessible from most areas within the LCA. The parks generally feature areas of open grass with variable intensity of tree planting which most commonly native species. Douglas Reserve, Kathleen Street Reserve and the Apex Street Park feature play equipment which encourages unstructured play in an outdoor environment. In addition to providing formal outdoor spaces Victor Avenue Park, Parkhurst Reserve, Peppermint Grove Reserve and Austin Reserve extend between blocks which increases the pedestrian permeability of the area.

The LCA is characterised as a residential area with commercial activity within the area generally limited to a small number of home businesses. There is a small commercial centre on the corner of Tower Street and Picnic Point Road adjacent to the north-west of the LCA. These provide a diverse range of services, specialist retail and casual dining options for the residents in the LCA, however residents will need to travel to the much larger Panania or Revesby Local Centres for a comprehensive range of retail, business services and hospitality options. The Revesby Presbyterian Church is located adjacent to the northern boundary and provides religious services to the area. The area is well serviced by a large number of privately run childcare and early learning centres located throughout the LCA. There are no schools within the LCA although the Panania Public is located adjacent to the western boundary, and the Picnic Point High School is located on the eastern boundary.

The area features a good degree of vehicular access from within the LCA to the surrounding area. Tower Street on the northern boundary connects the LCA to the Panania Local Centre to the west of the LCA. Picnic Point Road runs the length of the western boundary connects to Henry Lawson Drive to the south of the LCA, which links into the Sydney metropolitan road network. The rectilinear road network within the LCA features a reasonable degree of permeability allowing for relatively easy vehicle movement throughout the Area. The area is well serviced by busses with routes and multiple stops located on the boundaries of the area. These provide connections throughout the day to the Panania and Revesby Local Centres, where the T8 Trainline can access, as well as to central Bankstown.

Most of the longer streets have footpaths extending down at least one side of the street. However footpaths are rarely extended into the secondary streets leaving some areas without coverage. Where present footpaths are in reasonable condition, with some areas benefiting from extensive coverage of new footpaths which have been constructed in association with residential redevelopment. The topography of the area is generally flat which helps to facilitate ease of movement, however the long residential blocks can lead inflated travel distances for pedestrians. The quality of the tree planting varies across the LCA. Whilst some streets feature a good level of coverage, most streets feature large gaps wide gaps between trees. Planting is often on the opposite side of the street to the footpaths due to the narrow verges, whilst can improve the visual amenity of the environment it often leaves little shade protection for pedestrians.

5.9 EXISTING CHARACTER



Where older housing remains in the LCA they generally have a single storey built form, small footprint, low for open front fencing and they are setback within a landscaped setting.



There has been a substantial amount of redevelopment of increasingly diverse built forms and residential typologies within the LCA.

Additional driveways on to the street have reduced the amenity in some areas where on-street parking provision is reduced but the number of private vehicles are increasing..



Newly constructed two storey residences and dual occupancies often feature façade elements such as boxed balconies and extensive parapets creating bulky built forms that do not achieve the desired built form character of the LCA

They may also feature materiality such as high gloss cladding which contrast strongly with the existing pallet of materials used on the street.

5.9 FUTURE CHARACTER

DESIRED FUTURE CHARACTER

The South-East Panania and West Picnic Point Local Character Area has undergone a period of change in recent years due to the substantial amount of increasingly diverse residential redevelopment which has occurred throughout the LCA. These changes to the residential built form and fabric of the public realm have had a mixed impact within the LCA. This is most apparent where intensity or form of development that is incompatible with the existing character has reduced, rather than enhanced, the overall quality and amenity of the area.

The primary development type within the LCA should be low scale detached residential housing within a landscaped setting. Care needs to be taken when allowing higher density residential building types. Especially noting potential for these developments to impact the existing amenity or otherwise not reflect the existing character of the area. Where permitted these developments should be guided by the controls set out in Councils LEP and DCP. This will ensure the future built form outcomes are compatible with, and will enhance, the existing character of the area.

To ensure the low scale character of the area is maintained the maximum residential building height should be two storeys and allow for traditional medium-pitched roof forms. Future residential buildings should align with the prevailing front setback pattern on the street which is generous and allows for extensive open space forward of the building line.

To reduce apparent bulk in the building form new residential developments should feature modulated façades, incorporate shade elements, and feature habitable rooms with windows facing onto the street at ground level. They should avoid non-traditional roof forms, and elements such as large boxed balconies and extensive parapets as these can add undue bulk to the built form. Colours and materials should be selected to minimise the urban heat island effect, and to complement the existing character of the area.

Where front fencing is used to delineate front boundaries they should have a low height in order to maintain the open feel between the public and private domain. This will reinforce the open character of the streets which is a key characteristic of the LCA. Front yards should feature large areas of soft landscaping, and native tree planting should be encouraged to improve microclimate and enhance the existing tree coverage in the area.

Garages are to be located behind the front building line and set back from the front boundary to accommodate a parked vehicle fully within the lot boundary. Driveways are to be light in colour to reflect heat and minimize the heat island effect. New driveways should be single crossovers to minimise the prominence of the driveway and hard surfaces in the street and within front setbacks. Where multiple new driveways are proposed, most commonly seen on dual occupancy development, they should be located to retain on-street parking, minimise any reduction to the existing grass verged and preserve the street tree planting.

The existing vehicle network is a good quality and should be maintained. Opportunities to enhance the quality of the streetscape by increasing the extent and quality street tree planting, extending the footpaths into the secondary streets and increasing the permeability of large blocks should be pursued.

5.9 FUTURE CHARACTER



A compact built form and the incorporation of multiple materials on the façade multiple should result in a fine grained development that does not present undue bulk on the street. Open frontages with soft landscaping are to be encouraged.



The use of a good mix of materials and colours, deep eaves, a prominent entry, and a first floor setback behind the building will break down the bulk of a built form and create interest in the facade.

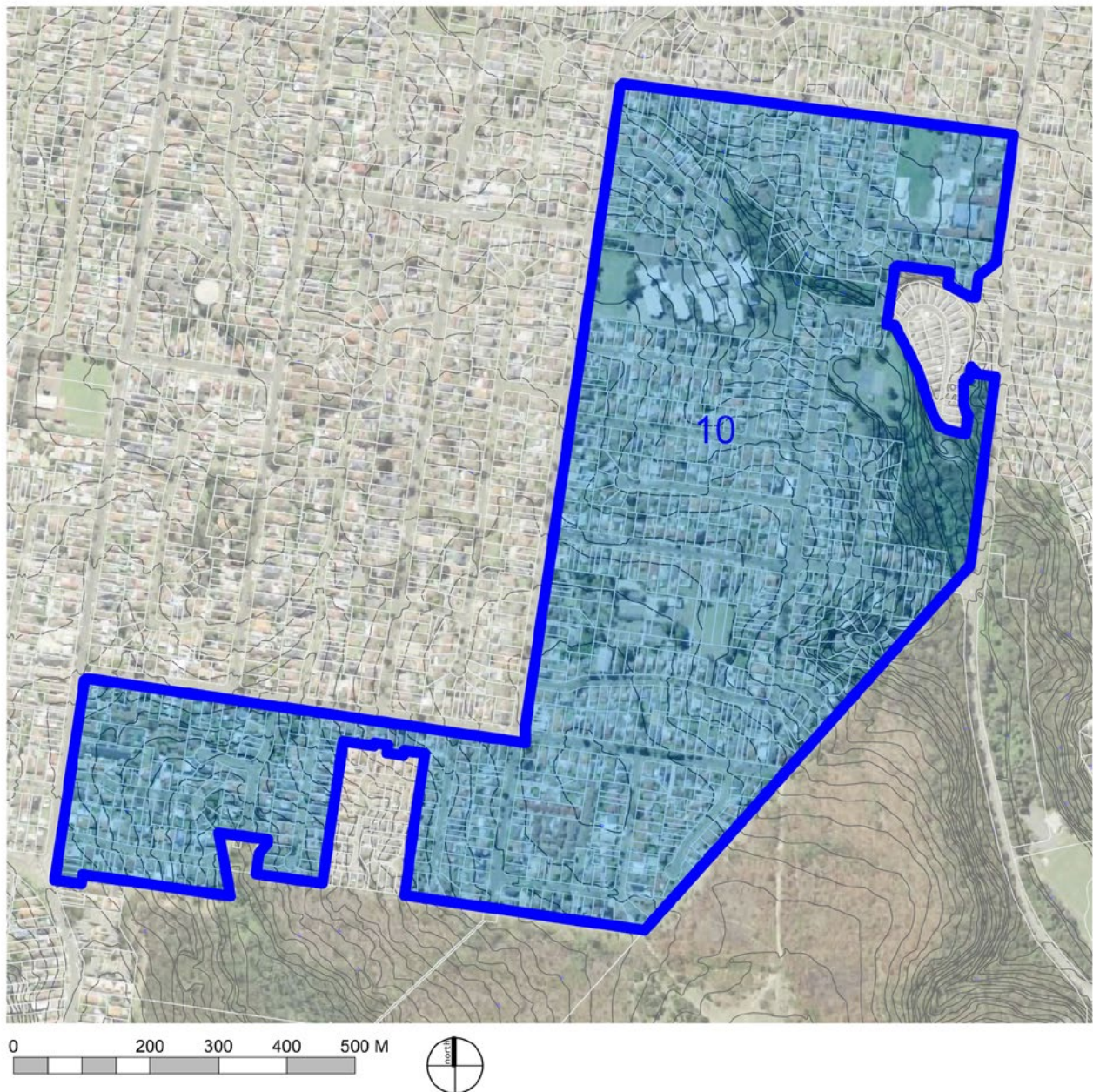
The setback garage, narrow driveway, open front boundary, and large areas of soft landscaping which should mature with dense shrub coverage will enhance the quality of the streetscape.

5.9 RECOMMENDED PROVISIONS

OBJECTIVES AND CONTROLS

Objective		Recommended Controls
O1 Enhance the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
	C2	Minimum lot size is to be 450 m ²
	C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
	C4	Minimum front setback is to be 5.5m
	C5	Maintain residential side setbacks consistent with the established street pattern.
	C6	Minimum side setback to be 0.9m clear of obstructions
O2 Enhance the public and private domain interface	C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
	C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
	C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
	C10	Maximum height of front fence to be 1.2m
	C11	Provide street trees of an appropriate native species and increased in numbers.
	C12	Provide footpaths to enhance pedestrian amenity
O3 Maintain the low scale of the LCA.	C13	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O4 Maintain the articulated built form which characterises the area.	C14	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
	C15	Provide a medium pitched primary roof form consistent with the area.
O5 Enhance the range of materials and colours.	C16	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
	C17	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
	C18	Provide materials and colours that are light or mid-tone and complementary with the existing finishes in the area.
O6 Minimise the visual impact of off street parking on the streetscape.	C19	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision
	C20	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
	C21	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

5.10 LOCAL CHARACTER STATEMENT



EAST PICNIC POINT

This Local character area is situated in the south- west of the Canterbury Bankstown LGA. The eastern boundary of the LCA is formed by The River Road and the Georges River National Park which returns around the LCA to forms the Southern Boundary of the Area. The stepped northern boundary is formed by Ferndale Road and Burns Road which are linked by Kennedy Street which forms the majority of the western boundary with Picnic Point Road marking the western-most point of the LCA. These streets mark the transition between the sloping bushland setting which characterises the area, to the suburban character of the Areas to the north and west of the LCA.

The LCA is located in an area of sloped topography where the land falls from the peak in the southeast of the LCA to the valleys and low points located to the north, east and west of the area. The road network features a generally with most streets maintaining an axial alignment layout despite the steep slopes in some areas. The topography provides for multiple opportunities for significant views, these are most easily gained from the high points in the area where the long streets provide for view regional across the

5.10 EXISTING CHARACTER

valley to the east and the suburbs west of the area. The relatively short streets to the south of Burns Road and in the east of the LCA provide regular views of the dense bushland in the National Park, reinforcing the natural setting of the LCA.

The majority of lots in the area have a rectilinear form and feature consistent lot widths which are aligned with the regular block structure. Irregular shaped lots are generally limited to the few curved streets within the LCA or on the end of cul-de sacs, with a limited number of battleaxe lots within the area. Lot widths within the area generally fall between 15-15.8m with lots most commonly between 550-600 sqm. There are a large number of lots sized between 600-700sqm these are often consistent with the prevailing lot width but feature larger lot depth. Lots above 700 sqm are generally the deepest lots in the area. The largest lots exceed 900 sqm these often lot widths of 18m with deep lot depths, or have irregular shapes. The smallest lots in the area have resulted from subdivision of larger lots to accommodate attached dual occupancy developments, these most often feature lot widths of approximately 7.5m and lots sizes of between 250 – 330 sqm. Residential setbacks are generally between 6-8m. While there are some individual residences with deeper setbacks, where variation occurs, it is most likely to be on newer housing setbacks ranging between 5-5.5m.

The area is low scale residential with building heights generally consisting of one and two storey built forms. In some instances where the land slopes steeply, housing can be substantially elevated above or below the street level, however this is generally restricted to lots in the east and west of the areas. The primary building typology is detached housing on single lots; however there has been a substantial amount of redevelopment of increasingly diverse residential development within the LCA. Whilst the amount of multi-residential development has generally remained static there are a rising number of attached dual occupancies within the area.

"Ferndale Cottage" is the sole heritage item within the area and it is located on Ferndale Road on the northern boundary to the LCA. The oldest housing in the area is predominantly detached single storey houses, with tiled pitched roofs and they generally either clad in lightweight material finished in red-brick masonry. Housing constructed in the late 20th century are a mix of one and two storey building heights with pitched tiled roofs and finished with masonry brick and may feature fully rendered façades. Housing constructed from the 21st century onward, including both detached dwelling and dual occupancy developments, are most likely to have a two storey building height, have a pitched roof form finished in tile or lightweight steel, and incorporate a mix of materials within the façade. Some of the newer housing, especially dual occupancy developments, feature extensive use of non-traditional elements for example boxed balconies, or incorporate extensive areas of render on large parapets. The elements can combine to create built forms of excessive bulk with visually prominent façades which are not compatible with the desired character of the LCA

Front boundaries are generally open or demarked with low planting, open fences or masonry walls finished in face brick or render. Some lots on sloping sites feature small retaining walls, although these are generally limited to streets in the north and south west of the area where the changes in the topography are most pronounced. Most front setbacks feature areas of lawns, and front gardens common throughout the area. The narrower lots, such as those developed as dual occupancies, have limited open space forward of the building line with these lots most likely to feature modest gardens within the front setback.

On the older properties in the area driveways are most often located to the side of the property and lead to open carports or continue to the rear of the property creating large separation between these houses and the side boundaries. The majority of detached housing constructed from the late 20th century onwards feature single or in many recent instances double width driveways leading to garages which are incorporated into the built form normally which are at, or in close proximity to the front building line. Attached dual occupancy developments generally feature single garages which feature both separated single driveways, or double driveways when garages are co-located in the centre of a lot. While most private properties feature a reasonable amount of space to allow for off-street parking of additional

5.10 EXISTING CHARACTER

vehicles, some streets are used extensively for private parking provision. This is particularly problematic in the vicinity of the schools in and where there are increased number of dual occupancies reduces the on-street parking provision whilst potential increasing the number of vehicles in the area.

The area contains a small number of reserves of varied size and character alongside the Georges River National Park surrounds the east and south of LCA. Samoa Reserve and Seidel Reserve are small open spaces with grassed areas and tree planting, with the later featuring play equipment and the former more suited to outdoor activities such as picnic and gatherings. The Thompson Street Tennis Courts support casual and formal sport and are located adjacent to the Morgans Creek Reserve which feature dense native planting. Amberdale Reserve on the southern boundary of the LCA provides access to the Georges River National park which contain walking trails that loop around the Yeramba Lagoon and connect to the Georges River south of the LCA.

The Area is primarily residential in character, with commercial activity generally restricted to the home businesses located in private residences. There is a small group of shops and casual dining venues located on the corner of Kennedy Street and Burns Road which provide a range of cuisines and some limited retail and beauty services to the LCA. However residents will need to travel to the nearby local Centres at Revesby and Panania to the north of the LCA for a full range of retail and business services. Whilst there are no early learning centres within the LCA, the area features a number of schools including the Picnic Point Public School, Picnic Point High School and the De La Salle Catholic College. The Saint Patrick's Catholic Church Provides religious services to the LCA.

The area features a good degree of vehicular access between the LCA and the surrounding suburbs. Picnic Point Road on the western boundary connects into Henry Lawson Drive, and to the suburbs north of the LCA. There are multiple access points to The River Road on the eastern boundary connects into the metropolitan road network and links the LCA to Central Revesby. Most of the residential blocks within the LCA are linked together by Kennedy Street which runs from the north to the south of the Area, or by Burns Road which runs from the west to the east of the LCA. The majority of streets are short and straight, with curves streets limited to areas with significant natural constraints. The allows for short travel times within a clear road hierarchy. The area is serviced by a single bus route which follows the north and western boundaries. This provides residents services throughout the day which connect to the T8 trainline at Panania and Revesby, with the route terminating at Central Bankstown.

The major roads in the areas and the streets around the schools feature footpaths in reasonable condition, however most of the shorter secondary streets lack footpath coverage. Whilst most streets feature verges of a reasonable width, narrow verges are not uncommon and are prone to obstruction by infrastructure and street tree planting, or parked vehicles. These tend to be the streets featuring steeper slope which can create a challenging environment for pedestrians to navigate in some locations. Streetlighting in the area is extensive throughout, although coverage on some of the secondary streets is limited. There is a reasonably high quality of street tree planting throughout the area providing a good level of protection to pedestrians. Whiles some streets feature gaps in coverage, most streets feature regular planting which are generally native species at low-mid height with some limited taller species.

5.10 EXISTING CHARACTER



Most streets feature detached housing from a range of different eras. Common elements between developments such as roof form, materiality and scale reinforce the character of the LCA.



Regional views out of the area are a key characteristic of the LCA and can be seen from many streets to the south of the area. These are normally framed by a diverse street edge where the range of development types has led to an increase in the number of driveways on the street. This can reduce parking provision on the often narrow streets.



Some residential developments have built form outcomes that are not compatible with the desired character of the LCA. Recessive entries, prominent, steep roof pitches and tall vertical can create excessively tall built form outcomes that are undesirable.

The quality of the streetscape can be reduced where developments have no window openings facing onto the street at ground level and prominent driveways.



Where older housing remains in the LCA they generally have a single storey built form, small footprint, low front fencing and they are set back within a landscaped setting which contributes to the open streetscape.

5.10 FUTURE CHARACTER

DESIRED FUTURE CHARACTER

The East Picnic Point Local Character Area features a diverse range of housing types, built forms and street addresses. This has created a heterogeneous character in some areas within the LCA with much of the recent residential development having a mixed impact on the overall quality of the public realm. This is most apparent where the intensity or form of development is incompatible with the desired character of the LCA. Increased intensity of development and vehicle numbers have reduced the overall amenity of certain parts of the area.

Future development within the area should enhance the existing low density residential character of area. The primary development type within the LCA should be low scale detached residential housing within a landscaped setting. Care needs to be taken when allowing higher density residential building types. Especially noting potential for these developments to impact the existing amenity or otherwise not reflect the existing character of the area. Where permitted these developments should be guided by the controls set out in Councils LEP and DCP. This will ensure the future built form outcomes are compatible with, and will enhance, the existing character of the area.

The maximum residential building height should be kept to two storeys however. Where development is on the sloping site within the LCA the built form is to be designed to step down or across the landform to lower bulk and scale. Front setbacks should be generous and consistent with the prevailing street setback. Front setbacks should feature soft landscaping and native tree planting should be encouraged to improve microclimate and extend the tree coverage within the area.

New buildings should avoid the use of large unbroken areas of monotonous materials and incorporate features to reduce apparent bulk and scale such as modulated façades and shade elements. Colours and materials should be selected to minimise the urban heat island effect, and to complement bushland setting of the LCA.

Front fencing, if present, should be low and open to promote the open nature of the streets which is a key characteristic of the area. Where retaining walls are required forward of the building line they should be as low as practicable and tiered to ensure the public domain is not enclosed.

Driveways should be single crossovers to minimise hard surfaces in front setbacks and maximise soft landscaping in the private domain. Where multiple new driveways are proposed, most commonly seen on dual occupancy development, they should be located to retain on-street parking, minimise any reduction to the existing grass verges and preserve the street tree planting.

Garages are to be located behind the front building line and allow sufficient clearance from the front boundary to accommodate a parked vehicle fully within the bounds of the property. Driveways are to be light in colour to reflect heat and minimize the heat island effect. On sites with steeply sloping landforms or natural constraints where this may not be possible, garaging should still be designed and located to not add undue height to the building form.

The existing vehicle network is a good quality and should be maintained. Opportunities to enhance the amenity of the streetscape by extending the footpaths into the secondary streets, by widening the narrow grass verges prone to obstruction, and infilling gaps in street tree coverage should be pursued.

5.10 FUTURE CHARACTER



Future residential developments should incorporate modulated façades. The use of different materials and finishes within the building fabric is to be encouraged as it can help to create a fine grain- low bulk built form.



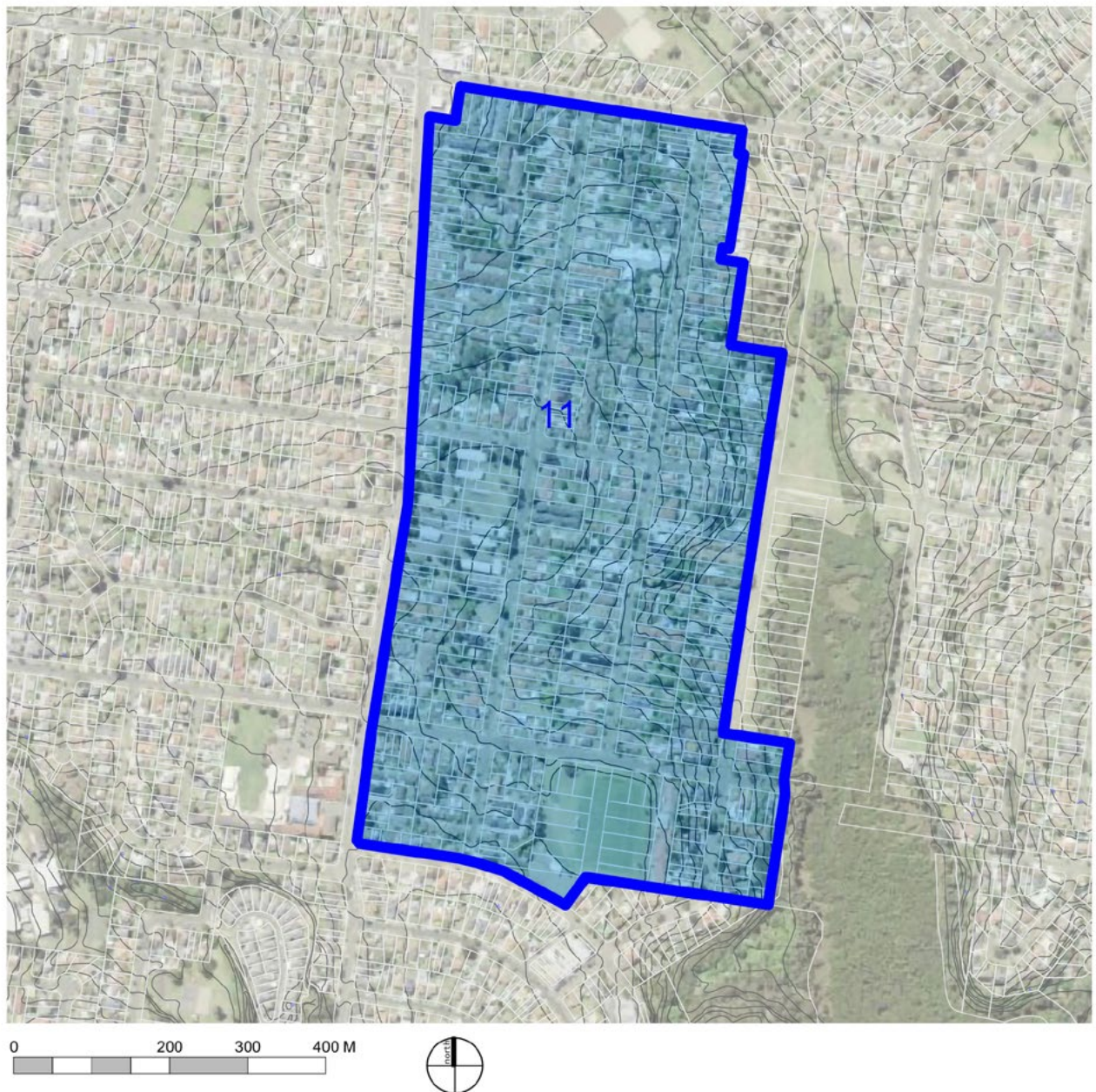
Future residential development should be at a height consistent with the desired low scale character of the area. Pedestrian entries should be located forward of the garage, front fencing should be low or open, and front setbacks should feature soft landscaping.

5.10 RECOMMENDED PROVISIONS

OBJECTIVES AND CONTROLS

Objective		Recommended Controls
O1 Enhance the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
	C2	Minimum lot size is to be 450 m ²
	C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
	C4	Minimum front setback is to be 5.5m
	C5	Maintain residential side setbacks consistent with the established street pattern.
	C6	Minimum side setback to be 0.9m clear of obstructions
O2 Enhance the public and private domain interface	C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
	C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
	C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
	C10	Maximum height of front fence to be 1.2m
	C11	Provide street trees of an appropriate native species and increased in numbers.
	C12	Provide footpaths to enhance pedestrian amenity
O3 Maintain the low scale of the LCA.	C13	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O4 Maintain the articulated built form which characterises the area.	C14	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
	C15	Provide a medium pitched primary roof form consistent with the area.
O5 Enhance the range of materials and colours.	C16	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
	C17	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
	C18	Provide materials and colours that are light or mid-tone and complementary with the existing finishes in the area.
O6 Minimise the visual impact of off street parking on the streetscape.	C19	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision
	C20	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
	C21	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

5.11 LOCAL CHARACTER STATEMENT



REVESBY SOUTH

This Local Character Area is located in the south-west of the Canterbury Bankstown LGA and encompasses the south-east of the suburb of Revesby. The western boundary is formed by The River Road and the south is bound by the suburb of Revesby. These two boundaries mark a change to where the character changes from higher density residential development on large deep lots to a finer grained lot patterns and an overall lower density of development the areas to the West and South of the LCA. The LCA is bound to the north by Uranus Road which separated the LCA from the Revesby Local centre, and Virginius Reserve and the suburb of Padstow for the eastern boundary.

The area is characterised by its rolling topography which transitions from the higher points in the south and west of the LCA down towards the north-east of the area where the topography flattens out. The LCA features clear block structure which has been established within a rectilinear road network that consists of long straight streets which terminate on the eastern and southern boundaries. The wide streets and elevation allow for regional views across suburbs to the east from some of the high points within the LCA.

5.11 EXISTING CHARACTER

The majority of residential lots within the LCA have a rectilinear form aligned to the road network. Lot widths within the Area are consistently between 15 -15.5m, with the exception being the block to the north-west of the LCA between The River Road and Varga Street where the lot widths range between 11-13m. The most common residential lots size range between 1000 – 1050sqm, however there are a number of lots with areas of between 600-750 sqm with the variation due to having a smaller lot depth than the most common larger lots in the LCA. The smallest lots in the area of 300-350 sqm and lot frontages of 7.5-8m are limited to sites that have been developed as dual occupancies. The limited number of irregular shaped lots often feature extensive multi-residential developments. These are generally located within the centre of the wider blocks and are sleeved by separated residential facing onto the street, so they do not alter the lot structure when viewed from the public domain. Front setbacks across the area are normally fall between 6-8m regardless of residential typology, with some limited variation on some streets where the average setbacks increase to 10m.

The primary residential typology in the area is detached dwellings on single lots. There are an increasing number of dual occupancy developments in the area and a similar number of multi-residential developments, however these cover a high percentage of the developable land and feature a higher intensity of dwellings than the other residential types. There are no heritage items within the LCA and the oldest housing in the areas are single storey detached housing with tiled pitched roofs finished lightweight cladding or red face brick façades. The majority of detached housing the LCA housing in the area features pitched roof forms at both one and two storey heights, these are most commonly finished in face brick with a limited number featuring fully rendered façades. The newest detached residential developments in the area are likely to have a two storey height and retain the pitched roof form, however they are likely to display a range of materiality across the different elements in each façade.

Recently constructed dual occupancy developments in the area are generally consistent in height, materiality and form to the overall character of the area especially that of the newer detached dwellings in the area. However some incorporate façade elements, roof forms, and materials which create outcomes that are inconsistent with the desired character of the area. The multi-residential developments in the area often feature a two storey built form, are finished with masonry brick and have a pitched tiled roof. They are often consistent with the front setback in the LCA, or area located mid-block and are not visible from the public domain. Overall most of these presents at a Scale and built form which is consistent with the low Scale low density residential character of the area.

Front boundaries in the LCA are demarked by a range of methods including low masonry or rendered walling and fencing, or planting on the front boundary line although a large number of dwellings feature open front boundaries. The majority of houses feature lawns forward of the building line and many properties feature extensive gardens forward of the building line. Dual occupancy developments and multi residential developments in the area are most likely to feature areas of soft landscaping and planting of within the front setbacks.

The off street parking provision is varied across the LCA with multiple configurations which vary based on the age as well as the type of residential development. On the oldest housing in the area, off street parking is often under an open carport located to the side of a residence or uncovered on the single width driveway. However the majority of detached residential housing incorporate garages into the building envelope. These are most often single garages on earlier developments, with newer detached housing featuring a mix of both single and double width garages setback which are generally connected to the street with a single width crossover.

Dual occupancy developments generally feature single garages which are most commonly located on opposing side of the built form. The multi-residential developments provide off street parking for residents but the majority do not feature on site parking provisions for visitors who are often forced to park on the street. While many front setbacks accommodate additional parked vehicles, some streets are heavily utilised for parking which is more apparent in the narrower streets in the area.

5.11 EXISTING CHARACTER

The area features two large formal outdoor spaces which are easily accessible from most points within the LCA. Neptune Park in the south of the area, is wide open space which feature a large playfield for formal sports, alongside a playground which allows for unstructured play. Little Virginius Reserve which runs the length of eastern boundary. The large reserve incorporates Salt Pan Creek and features of spaces of differing character including formal landscaped areas, area of dense tree planting.

The character area does not contain any retail provision with the majority of commercial activates within the LCA taking the form of home businesses. However the Revesby Local Centre to the north of the LCA provides a wide range of retail, casual dining and services. There are features a the Revesby South Public School, and there is one kindergarten in the south-west of the LCA. The Revesby Uniting church provides religious services, and the Yallambee Village provides residential aged care facilities to the area.

The clear axial street network provide for relatively unencumbered travel by vehicle across the LCA. The multiple exit points in the west and north the LCA providing connection to the River road which links into the Sydney metropolitan road network, and Uranus road which provides more localised connections to the adjoining suburb. There is good degree of access to public transport services in the LCA . The well spaced bus stops provide access to the bus route that loops through the LCA and provides access to central Revesby, the Padstow Local Centre and to Central Bankstown. There are a large number of additional bus services that can be accessed from the bus stops located along the Western boundary which link the area to wide variety of local and greater Sydney destinations.

The area features a high quality pedestrian environment which facilitates ease of movement though the LCA. The majority of the streets feature footpaths down at least one side. Street lights are common however can be spaced to facilitate vehicle movement rather than to illuminate the pedestrian environment. The wide grasses verges support an extensive network of street tree planting. These are a mix of mature low and mid rise native species which generally provide a good degree of shade coverage to the street.



Older buildings within the LCA have a compact form and narrow driveways and substantial soft landscaping which contributes to the public realm



The increased intensity of development has, in some areas reduced the overall quality and amenity of the public realm.



Some areas feature a large number of driveway crossings and vehicles in the front setbacks which can lead to teh vchile taking primacy in the street.



Open front setbacks, the large number of street trees and views down the long tree lined streets are important characteristic of the area.

5.11 FUTURE CHARACTER

DESIRED FUTURE CHARACTER

The South Revesby Local Character Area features a diverse range of housing types, built forms and street addresses which have combined to form a heterogeneous character across much of the LCA. This is most evident where the intensity of development has had a mixed impact on the overall quality of the public realm. Increased intensity of development and vehicle numbers have reduced the overall amenity of certain parts of the area.

Future development within the area should enhance the existing low density residential character of area. The primary development type within the LCA should be low scale detached residential housing within a landscaped setting. Care needs to be taken when allowing higher density residential building types. Especially noting potential for these developments to impact the existing amenity or otherwise not reflect the existing character of the area. Where permitted these developments should be guided by the controls set out in Councils LEP and DCP. This will ensure the future built form outcomes are compatible with, and will enhance, the existing character of the area.

To ensure the low scale character of the area is maintained the maximum residential building height should be two storeys and allow for traditional medium-pitched roof forms. Future residential buildings should align with the prevailing front setback pattern on the street. This allows for a good degree of open space forward of the building line which should include gardens and soft landscaping which be encouraged in future development.

To reduce apparent bulk in the building form new residential developments should feature medium pitched primary roof forms consistent with the area, incorporate multiple materials within modulated façade and feature habitable rooms with windows facing onto the street at ground level. New dwellings should avoid non-traditional roof forms and elements such as large boxed balconies and extensive parapets as these can add undue bulk to the built form. Colours and materials should be selected to minimise the urban heat island effect, and to complement the existing character of the area.

Where front fencing is used to delineate front boundaries they should have a low height in order to maintain the open feel between the public and private domain. Front yards should feature large areas of soft landscaping and large areas of hard surfaces such as paving are to be avoided. Native tree planting should be encouraged to improve microclimate and enhance the existing tree coverage in the area.

New driveways should be single crossovers to minimise the prominence of the driveway and hard surfaces in the street and within front setbacks. Where multiple driveways are proposed they are to be located to retain on-street parking and are not reduce the existing street tree planting. Driveways are to be light in colour to reflect heat and minimize the heat island effect. Garages are to be located behind the front building line and set back from the front boundary to accommodate a parked vehicle fully within the lot boundary.

Both the existing vehicle network are good quality and should be maintained. Opportunities to enhance the pedestrian environment by extending footpath coverage and increasing the permeability of large lots with new pedestrian links should be explored by council.

5.11 FUTURE CHARACTER



Future residential developments should incorporate modulated façades. The use of different materials and finishes on different elements is to be encouraged.



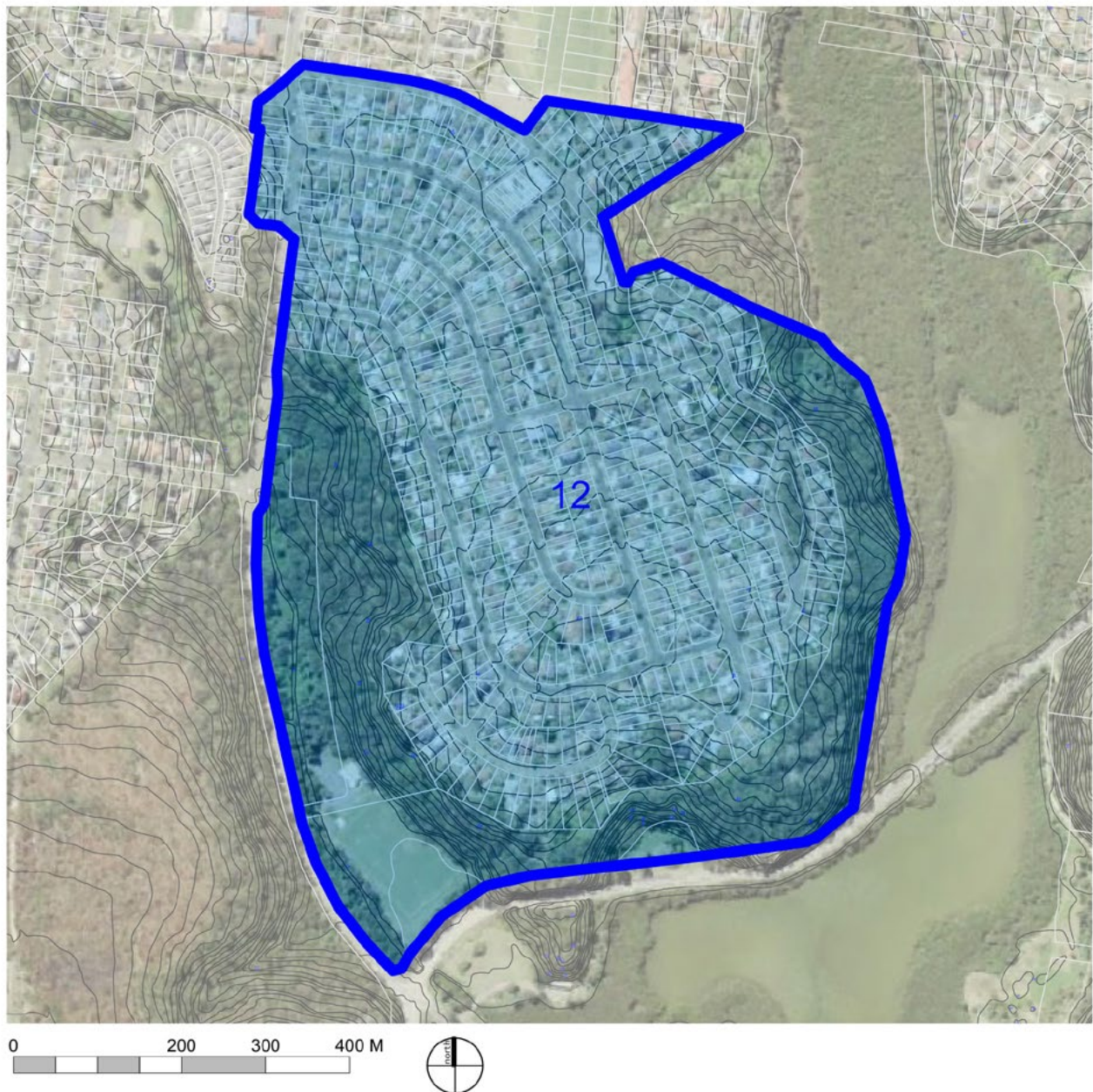
Future residential development should enhance the public domain and street edge by featuring areas of soft landscaping with dense planting forward of the building line.

5.11 RECOMMENDED PROVISIONS

OBJECTIVES AND CONTROLS

Objective		Recommended Controls
O1 Enhance the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
	C2	Minimum lot size is to be 450 m ²
	C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
	C4	Minimum front setback is to be 5.5m
	C5	Maintain residential side setbacks consistent with the established street pattern.
	C6	Minimum side setback to be 0.9m clear of obstructions
O2 Enhance the public and private domain interface	C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
	C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
	C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
	C10	Maximum height of front fence to be 1.2m
	C11	Provide street trees of an appropriate native species and increased in numbers.
	C12	Provide footpaths to enhance pedestrian amenity
O3 Maintain the low scale of the LCA.	C13	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O4 Maintain the articulated built form which characterises the area.	C14	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
	C15	Provide a medium pitched primary roof form consistent with the area.
O5 Enhance the range of materials and colours.	C16	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
	C17	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
	C18	Provide materials and colours that are light or mid-tone and complementary with the existing finishes in the area.
O6 Minimise the visual impact of off street parking on the streetscape.	C19	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision
	C20	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
	C21	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

5.12 LOCAL CHARACTER STATEMENT



REVESBY HEIGHTS

This Revesby Heights Local Character Area is located adjacent to the southwest boundary of the Canterbury Bankstown LGA. Henry Lawson drive forms the southern boundary of the LCA and the suburb of Revesby forms the northern boundary to the Area. The LCA is bound to the east by the Little Salt Pan Creek which separates the LCA from Padstow Heights. The western boundary is formed by Morgan's Creek and The River Road which cuts through the Georges River National Park to the west of the Area.

The area is located atop a ridge which runs along the centre of the LCA and rises from the north to the peak near the south of the LCA adjacent Alamein Road. From there the land slopes down towards the western, southern and eastern boundaries where the rate of fall increases, and the steep areas coincide with the transition from residential development to areas of bushland and parks. The curvilinear roads network follows the slope of the area and the well structured layout which supports regular pattern of residential lots. The high points in the area allow for some limited regional views out of the LCA, the dense tree canopy which surrounds the LCA can be seen from most streets within the LCA.

5.12 EXISTING CHARACTER

The residential lots generally have a regular pattern of subdivision pattern aligns with the road network. Some lots feature irregular shapes; however these are few in number and mostly located on cul-de-sacs or adjacent to the boundaries at the east and south of the LCA. Lot widths within the LCA generally fall between 15-15.6m and while some wider lots exist, these are few in number. The majority of lot have site areas of between 500 – 600m. There are a number of larger lots of between 600 – 700 sqm which are generally the result of an increase to lot depth than variation to lot width. This also holds true to the largest lots at the south of the LCA which feature much larger depths than the majority of lots within the Areas. There are a large number of small lots of between 250 – 300 sqm with 7.5m lot widths within the LCA. These are the result of the increasing number of attached dual occupancy development which has been occurring within the area.

Residential front setbacks are generally between 5.5 -7.5. Some lots feature larger setbacks of upto 9-10m however these are generally only seen on lots featuring older houses, with a limited number houses feature smaller setbacks.

The area is characterised as a low density residential area and the predominant typology is generally either one or two storey detached housing on single lot. However there has been a substantial amount of redevelopment of increasingly diverse residential development which has expanded to include two storey attached dual occupancies which are becoming a common typology within the area. The topography often influences the siting of individual dwellings with some ground floors elevated above the street level.

There are no heritage items within the LCA and the older housing are generally feature tiled roofing and are clad in lightweight weatherboard and finished in light colours. Developments from the late 20th century are generally finished with masonry brick, and feature pitched roofs which can be lined with roof tiles or lightweight steel. The newest residences from the 21st century onwards are more likely to incorporate non-traditional roof forms, balconies on the front façade, and a wide range finishes including large render and cladding metallic finishes. Whilst the outcomes are generally compatible with the existing character of the LCA. The bulk, Scale and materiality of some built form outcomes, most commonly on dual occupancy developments, are not compatible with the desired character of the LCA

Front boundaries throughout the LCA are generally low and open, with walling finished in the a style which reflects the architecture of the adjacent house. Older housing is most likely to have open from yards which seamlessly transition between the public and public domain. Where newer residential developments demark front boundaries, these are generally low walls of render or masonry brick. Front setbacks generally feature extensive areas of grass, and while some properties feature extensive front gardens, most often planting is at a low Scale and relatively modest.

Off-street parking on lots with older detached housing are often under open carports or on uncovered driveways and hardstands both in front, or to the side of the houses. Most recent developments feature garages that are incorporated into the primary building envelope. Attached dual occupancy developments generally feature single garages which feature both separated single driveways, or double driveways when garages are co-located in the centre of a lot. Detached residences are most likely to feature double garages with corresponding wide driveways and street crossovers. While these can become a prominent façade feature the this is somewhat offset by the wide areas lawn and soft landscaping on many lots. Despite the large amount of parking provision in the private domain, on street parking is common in some streets which can restrict car movement.

There LCA is notable for its Bushland setting which surround the LCA with the majority of the parks and reserves are located within this area of extensive natural planting . Boomerang Reserve located along the southern boundary of the LCA support a range of outdoor activates, while multiple access points between residential lots in the south of the LCA facilitate pedestrian access, to topography may travel via vehicle the preferred method of access. The Gurawak Fire Trail provides opportunity for bushwalking through a rugged nature reserve. The Bill Delauney Reserve at the southwest of the LCA plays host to formal sport and is capable of hosting a wide range of outdoor activities.

5.12 EXISTING CHARACTER

However there are a small number of parks and reserves within the LCA including the Cox Park in the east of the LCA which has been densely planted with native vegetation and is a space for quiet reflection alongside the Heroes Hill Memorial. The Boomerang Reserve located along the southern boundary of the LCA support a range of outdoor activities, while multiple access points between residential lots in the south of the LCA facilitate pedestrian access, to topography may travel via vehicle the preferred method of access. The Gurawak Fire Trail provides opportunity for bushwalking through a rugged nature reserve. The Bill Delauney Reserve at the southwest of the LCA plays host to formal sport and is capable of hosting a wide range of outdoor activities.

The area is primarily residential, and while there is a very limited number of home businesses in the LCA. There is a café and a small run of businesses with shopfronts located on Centaur Street. These provide bespoke services generally focused on health and beauty. To access a comprehensive range of grocery, retail and will need to travel out of the LCA to Revesby Local Centre. The area does not include any schools or childcare centres, however the De La Salle Catholic College and St Patrick's Catholic Church are located adjacent to the north west boundary.

The small size of the area combined with the clear road hierarchy means that travel distances and times across the LCA via vehicle are short. There are multiple access points onto The River Road at the north west of the LCA which connects the LCA to the Revesby Local centre to the north of the Area. A single bus route runs through the area which links the area to central Revesby and continues on to central Bankstown. The bus provides regular services throughout the week which run from morning to late evening and bus stops are easily accessible from most parts of the LCA.

The overall walkability of the area is mixed, while the permeability of the area is high, this is offset by factors including the location and extent of footpaths, some areas of sloping topography and the amount of street trees and limited overhead lighting on the secondary streets. It is common for areas within the LCA to not feature footpaths, this is most often seen on the streets with sloping topography, which can make walkability challenging, especially for those requiring assistance to move through an area. Where footpaths are present, they are often only down one side of the street, these are generally in reasonable condition although some sections may present a hazard to those moving through the LCA. Grass verges are common throughout the LCA, however the amount of street tree planting within the areas is limited. Where planting is present the species are a mix of native species with compact canopies, although these are at a range of maturity and are often located on the opposite side of the street to the footpaths. Street lighting is generally located to benefit vehicle movement rather than to increase pedestrian amenity.

5.12 EXISTING CHARACTER



There has been a substantial amount of redevelopment of increasingly diverse built forms and residential typologies within the LCA.

The often narrow streets can be congested due to the increase in car numbers alongside a reduction in the amount of on-street parking provision.



Where older housing remains in the LCA they generally have a single storey built form, small footprint, low for open front fencing and they are setback within a landscaped setting which contributes to the open streetscape.



Regional views out of the area are a key characteristic of the LCA and they should be protected and maintained.



Most streets feature detached housing from a range different eras. Common elements between new and older housing such as roof form, materiality and scale can to reinforce the character of the LCA. However this can be lacking in some instances where there are shifts in scale, setbacks, form and between old and new developments.

5.12 FUTURE CHARACTER

DESIRED FUTURE CHARACTER

The Revesby Heights Local Character has undergone a period of change in recent years due to the substantial amount of increasingly diverse residential redevelopment which has occurred throughout the LCA. These changes to the residential built form and fabric of the public realm have had a mixed impact within the LCA. This is most apparent where the built form outcomes on new residential development has contrasted with the desired character of the area. Increased intensity of development and vehicle numbers have reduced the overall amenity of certain parts of the area.

Future development within the area should enhance the existing low density residential character of area. The primary development type within the LCA should be low scale detached residential housing within a landscaped setting. Care needs to be taken when allowing higher density residential building types. Especially noting potential for these developments to impact the existing amenity or otherwise not reflect the existing character of the area. Where permitted these developments should be guided by the controls set out in Councils LEP and DCP. This will ensure the future built form outcomes are compatible with, and will enhance the existing character of the area.

To ensure the low scale character of the area is maintained the maximum residential building height should be two storeys and allow for traditional medium-pitched roof forms. On the steepest sloping sites buildings should adapt to the land form and feature stepped built forms to minimize overall height and scale. The existing pattern of front setbacks should be maintained in most instances. However on the streets which have a small average setback pattern, future development should feature a greater front setback. This will increase the average setback from the street to enhance the streetscape by ensuring there will be space forward of the building line for soft landscaping.

Future development should look to minimize the bulk of the built form by incorporating modulated façades, the use of different materials and finishes within the building fabric, and feature medium pitch roofs. The incorporation of elements such as large boxed balconies, extensive parapets, and non-traditional roof forms should be avoided in future development as these can add undue height and bulk to the building form. Colours and materials should be selected to minimise the urban heat island effect, and to complement the bushland setting of the LCA.

Where front fencing is used to delineate front boundaries, it should be low walling or open fencing consistent with the character of the area. Front yards should feature large areas of soft landscaping, and native tree planting should be encouraged to improve microclimate and enhance the existing tree coverage in the area.

New driveways should be single crossovers to minimise the prominence of the driveway and hard surfaces in the street and within front setbacks. Where multiple driveways are proposed, such as on dual occupancy development, they should be located to retain on-street parking which will maximise the remaining area of grassed verges and street tree planting. Driveways are to be light in colour to reflect heat and minimize the heat island effect. Garages are to be located behind the front building line and set back from the front boundary to accommodate a parked vehicle fully within the lot boundary. On the steeply sloping sites within the LCA, garaging should be designed and located to avoid adding undue height to the building form, and ensure that where possible it remain a passive element in the façade.

The existing vehicle network is of a good quality and it should be maintained. Council should look to improve the amenity for residents by extending the footpath coverage and increasing the currently low amount of street tree planting in the public domain.

5.12 FUTURE CHARACTER



Future residential developments should incorporate modulated façades. The use different materials and finishes within the building fabric is to be encouraged as it can help to create a fine grain- low bulk built form.



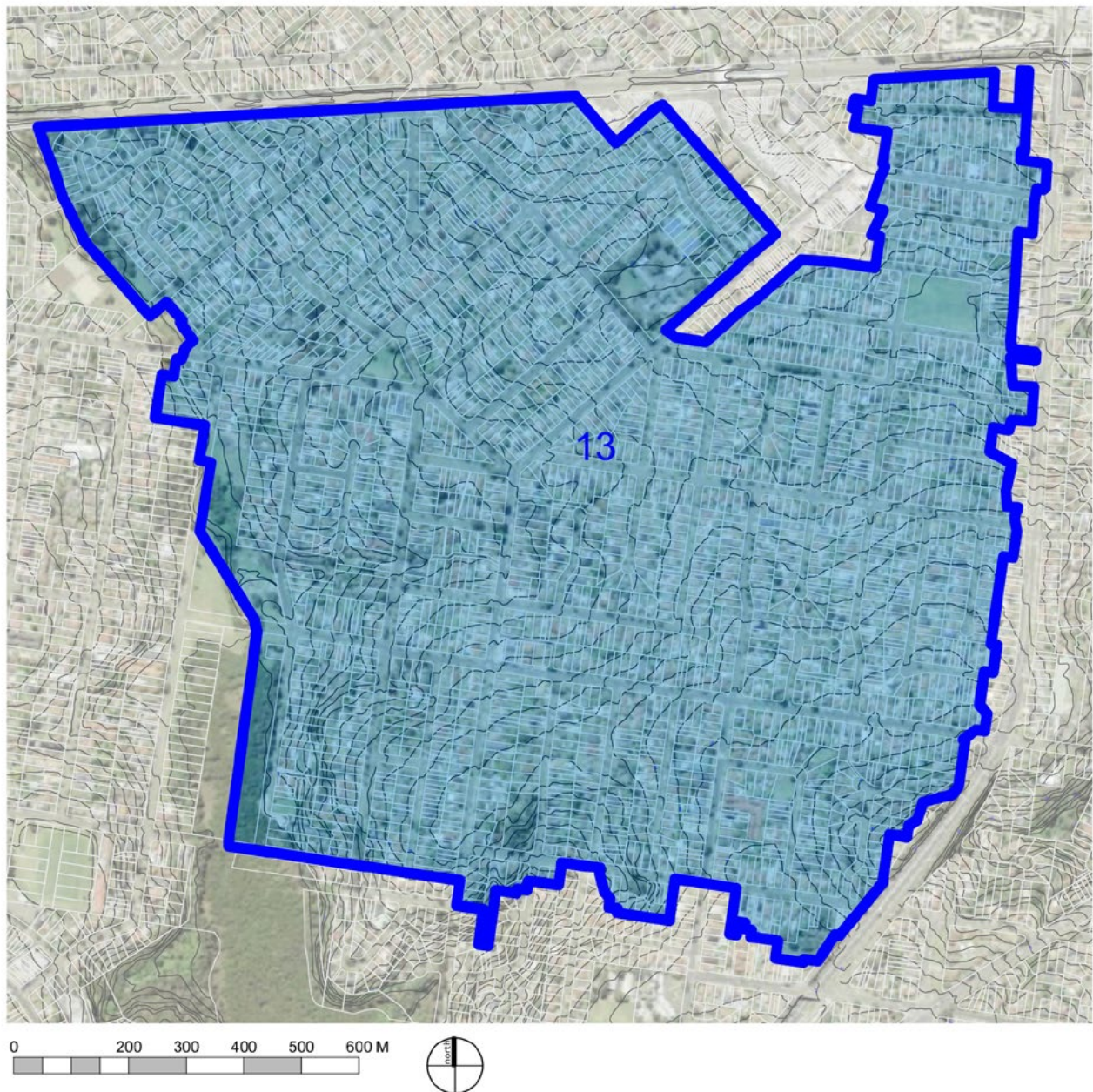
Future residential development should be at a height consistent with desired low scale character of the area. Pedestrian entries should be located forward of the garage, front fencing should be low or open, and front setbacks should feature soft landscaping.

5.12 RECOMMENDED PROVISIONS

OBJECTIVES AND CONTROLS

Objective		Recommended Controls
O1 Enhance the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
	C2	Minimum lot size is to be 450 m ²
	C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
	C4	Minimum front setback is to be 5.5m
	C5	Maintain residential side setbacks consistent with the established street pattern.
	C6	Minimum side setback to be 0.9m clear of obstructions
O2 Enhance the public and private domain interface	C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
	C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
	C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
	C10	Maximum height of front fence to be 1.2m
	C11	Provide street trees of an appropriate native species and increased in numbers.
	C12	Provide footpaths to enhance pedestrian amenity
O3 Maintain the low scale of the LCA.	C13	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O4 Maintain the articulated built form which characterises the area.	C14	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
	C15	Provide a medium pitched primary roof form consistent with the area.
O5 Enhance the range of materials and colours.	C16	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
	C17	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
	C18	Provide materials and colours that are light or mid-tone and complementary with the existing finishes in the area.
O6 Minimise the visual impact of off street parking on the streetscape.	C19	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision
	C20	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
	C21	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

5.13 LOCAL CHARACTER STATEMENT



PADSTOW

This Local Character Area is located in the south of the Canterbury Bankstown LGA. The northern boundary is formed by the T8 Trainline and the commercial core of the Padstow Local Centre. The south of the LCA is bound by the Suburb of Padstow Heights and Davies Road runs the length of the eastern boundary. The western boundary is a combination of the line of Little Salt Pan Creek and the boundary between the LCA and the suburb of Revesby.

The area generally slopes from the high points in the east and north of the area towards the south and the west of the LCA. While the falls in the east and north of the LCA are gradual, slopes can extend long distances with the changes in elevation most apparent on the longer streets in the area. The steepest slopes are located nearest to the western and southern boundaries where the rate of fall increases as the topography transitions down into the waterways and valleys which bound the LCA. The road network within the Area has a rectilinear form across most of the LCA. As most of the LCA is elevated above the surrounding areas, regional views to the east, west and south can be seen from several locations within the LCA. Many of the streets close to the southern and western boundaries also provide views of the native bushland located to the west of the area.

5.13 EXISTING CHARACTER

The residential lots generally feature a rectilinear shape aligned with the axial road network. There are some irregular lots these are generally not apparent from the street, as the variations are often at the rear of a lot. The LCA includes lots with a diverse range of sizes however the majority of residential lots in the area have an area of between 500 – 650 sqm. There are a number of smaller lots in the area, these have a size of between 400 – 500 sqm and are most often located on the short ends of blocks. The smallest lots in the LCA have an area of between 250- 350 sqm which are the result of subdivision of larger lots to accommodate dual occupancy development. The largest lots in the area of 700 – 800 sqm are generally the widest and deepest lots within the LCA.

Lot widths tend to be narrow and fall between 12-14.5m, however there is variance across the LCA with some areas featuring groups of lots which deviating from the norm. Lots with widths of 15-15.5m are located throughout the LCA, however they are most likely to be located towards the centre and in the east of the LCA. The widest lots in the area of 16-18m are concentrated within the blocks south of Alma Road. Narrower lots of between 7.5-8.5m are restricted to lots featuring dual occupancies and are distributed across the LCA. The average front setbacks across the LCA are generally between 6-8m. In areas in the east and south of the LCA this increases to 7.5-10m where there is a combination of wider lots and the oldest housing on some streets.

The area features a mix of residential typologies with one and two storey detached residential housing on single lots being the most common. There are an increasing intensity of dual occupancy developments throughout the Area, and a number of multi-residential developments which are located towards the west and south of the LCA. Some areas of sloping topography can affect the height of buildings relative to the adjacent properties where houses step down to follow the land form, or relative the public domain with some elevated above and below the street in order to adapt to the changing landforms.

There are no heritage items within the LCA and the oldest housing in the area are increasingly being replaced with newer larger Scale housing. The oldest developments are smaller houses constructed post WWII. These feature wide setbacks, feature pitched tiled roofs, and are most often clad in lightweight or weatherboard, with some examples constructed with red face bricks. Late 20th century housing developments are a mix of one and two storey developments with tiled pitched roofs, most commonly finished in face brick in a range of hues, with a limited number finished with rendered façades. 21st century detached residential development are most likely to have a two storey built form with a pitched roof, and tend to incorporate multiple materials into the façade treatments which are most often a mix of face brick and rendered elements.

Recently Constructed dual occupancy developments in the area are generally consistent in height, materiality and form to the overall character of the area, however they are more likely to incorporate elements which add to the bulk and Scale of the built form such as large areas of boxed balconies, parapets and non-traditional cladding elements which sometimes contrast sharply with the residential character of the area. The multi-residential dwellings in the area have been constructed over a number of eras but generally feature masonry façade and tiled pitched roofs. They are often located on deep lots, comply with the prevailing front setbacks, and present at a Scale and built form consistent with the low density and Scale residential character of the area

Front fencing is consistently low masonry or open fencing on the busiest roads in the LCA. On the secondary streets in the area the number generally feature a large number of properties with planting or no front fencing alongside those with low walling on the front boundary line. Some sloping sites feature low to mid height stone masonry or brick retaining walls on the front boundary. The majority of front setbacks feature large areas of soft landscaping, and open areas of grass are common, including sloping sites where lawns generally follow the land form, or are incorporated into the tiered areas in the setbacks forward of some residences. Front gardens are common throughout the LCA ranging from more modest on dual occupancy developments, and to the north and east of the LCA to more substantial which are most often seen in the south of the LCA or on sloping sites.

Off-street parking is commonly located to the side of the oldest housing, with single crossovers leading to open carports or uncovered driveways. Housing from the late 20th onwards incorporate garages

5.13 EXISTING CHARACTER

within the building envelope, these are generally single garages on the older of these developments, with double width garages progressively more common on the newest detached residential dwellings often constructed from the 21st century onwards. Dual occupancy developments generally feature single garages located to the side, or co-located in the middle of the subdivided lots. This can result in a mix of driveway configurations from well spaced single driveways to wider double with crossovers in the centre of a subdivided lot. The housing in the area generally feature setbacks that allow space for the parking of at least one additional vehicle wholly within the building lot. However, many streets are used extensively for parking of additional vehicles. While some of the wider roads have been marked for parking, many of the narrower secondary streets can become congested with parked cars reducing the effective road width. Or grass verges are used to supplement on-street parking provision. This is often seen where the intensity of dual occupancy development increases the potential number of vehicles in an area whilst reducing on street parking provision.

The area is generally well serviced by parks and reserves of different character and size which can support a wide range of activities. Little Salt Pan Creek Links Montgomery Reserve on the north western corner of the site with Virginius Reserve which runs most of the western boundary. These parks provide a series of spaces which include formal landscaped areas, large areas of open grass, and substantial tree planting alongside a range of public facilities. Richardson Reserve in the southwest of the LCA features a series of small open spaces set amongst extensive planting and tree coverage. Fox Reserve in the north west, and Starr Reserve towards the south east are the smallest reserves in the LCA, which feature modest Playground and some tree planting. Clark reserve to the north of the Area features sports fields and flood lights which can support a range of formal sports for extended hours.

The area is primarily residential, with commercial activity generally limited to the small selection of specialist shops and casual dining venues on Alma Road, or in the number of home businesses in the area. The Panania Local Centre is located adjacent to the northern boundary providing residents with a wide range of retail and business services including the Padstow Library and Knowledge Centre. The Padstow RSL on the northern boundary provides a range of entertainment, recreational activities and hospitality services, alongside the range of venues located within the Local Centre. There are three privately run childcare centres in the Area, two in the north-west, and one towards the south-east of the LCA. Padstow Public school, and St Therese Catholic Primary school provide formal educational services in the area. St Therese Catholic Church is co-located with the school by the same name, and along with the Padstow Congregational Church, provide religious services to the area.

The Area has good regional connection with multiple exit points onto Davies Road which connects into the wider Sydney metropolitan road network. Uranus Road provides connection to Revesby and the suburbs west of the LCA. The extensive road network within the LCA features a good degree of permeability. This allows for relatively easy vehicle movement through the LCA, although some streets can become congested with traffic during peak times. Some residents in the south west of the area are potentially underserved by public transport as they have to travel longer distances over sloped areas to access bus services. However the majority of residents are able to easily access public transport services, with the number of services and route options higher in the north of the LCA. adjacent to Central Padstow where the Padstow train station is regularly serviced by the T-8 Airport line.

The extensive grassed verges support street that are a mix of low and mid height native species with varying quality of canopy. While the amount of street tree planting across the area is reasonable, the number and spacing of tree planting varies by street, and sometimes within the length of a street. The quality and extent of shade coverage can be mixed as a result. Street lights are common on the busiest streets, however on the secondary streets they are generally spaced to facilitate vehicle movement and some areas lack coverage. While the area is fairly permeable the quality of the footpaths and extent of the coverage is variable. The major road and the areas around the Schools featuring footpaths down both sides of the street. The majority of blocks feature footpaths down one side, while streets especially those terminating in cul-de-sacs do not feature footpaths. This can create a challenging pedestrian environment on some streets, especially where there is a lack footpaths combined with sloping terrain. This may make individuals seek alternates to active transport especially when individuals want to travel the long distances across the length of LCA.

5.13 EXISTING CHARACTER



Views across the LCA are a key characteristic of the area and should be protected. Where older housing remains in the LCA they generally have a single storey built form, small footprint, low for open front fencing and they are setback within a landscaped setting which contributes to the open streetscape.



There has been a substantial amount of redevelopment of increasingly diverse built forms and residential typologies within the LCA.

The often narrow streets can be congested due to the increase in car numbers alongside a reduction in the amount of on-street parking provision.



Most streets feature detached housing from a range of different eras. Common elements between new and older housing such as roof form, materiality and scale can reinforce the character of the LCA.



Newly constructed two-storey residences and dual occupancies often feature façade elements such as boxed balconies and extensive parapets that create bulky built forms that do not achieve the desired built form character of the LCA.

They may also feature materiality such as high gloss cladding which contrasts strongly with the existing palette of materials used on the street.

5.13 FUTURE CHARACTER

DESIRED FUTURE CHARACTER

The Padstow Local Character Area has undergone a period of change in recent years due to the substantial amount of increasingly diverse residential redevelopment which has occurred throughout the LCA. These changes to the residential built form and fabric of the public realm have had a mixed impact within the LCA. This is most apparent where intensity or form of development that is incompatible with the existing character has reduced, rather than enhanced, the overall quality and amenity of the area.

The primary development type within the LCA should be low scale detached residential housing within a landscaped setting. Care needs to be taken when allowing higher density residential building types. Especially noting potential for these developments to impact the existing amenity or otherwise not reflect the existing character of the area. Where permitted these developments should be guided by the controls set out in Councils LEP and DCP. This will ensure the future built form outcomes are compatible with, and will enhance, the existing character of the area.

To ensure the low scale character of the area is maintained the maximum residential building height should be two storeys and allow for traditional medium-pitched roof forms. Future residential buildings should align with the prevailing setback pattern on the street to reinforce this characteristic of the area. Front setbacks should be generous and continue with the prevailing setback pattern. They should contain minimal hard surfaces forward of the building line and feature areas of soft landscaping and gardens which will enhance the street edge and public domain.

To reduce apparent bulk in the building form new residential developments should feature modulated façades, feature shade elements, and incorporate multiple materials within the façade treatment. New residential developments should not feature extreme roof pitches, excessively bulky façade elements, the use of high gloss finishes or large unbroken areas of monotonous materials. Colours and materials should be selected to minimise the urban heat island effect and to enhance the existing character of the area.

Where front fencing is used to delineate front boundaries they should have a low height in order to maintain the open feel between the public and private domain which will reinforce the open character of the streets. Front yards should feature large areas of soft landscaping, and native tree planting should be encouraged to improve microclimate and enhance the existing tree coverage in the area.

New driveways should be single crossovers to minimise the prominence of the driveway and hard surfaces in the street and within front setbacks. Where multiple driveways are proposed they should be located to retain on-street parking which will maximise the remaining for grassed verges and street tree planting. Driveways are to be light in colour to reflect heat and minimize the heat island effect. Garages are to be located behind the front building line and set back from the front boundary to accommodate a parked vehicle fully within the lot boundary. On the sloping sites within the LCA garages should, where possible, be located to ensure they remain a recessive elements within the façade and do not add undue height to the built form.

The existing vehicle network is extensive and of a reasonable quality and should be maintained. Opportunities to improve the streetscape by infilling gaps in street tree coverage and by extending the footpath network within the LCA should be pursued by council.

5.13 FUTURE CHARACTER



New two storey residence with an open front garden with a garage setback from the front façade and a medium pitched roof. Multiple façade elements create a modulated building form.



The use different materials and finishes within the building fabric is to be encouraged as it can help to create a fine grain-low bulk built form.



Future residential development should be at a height consistent with desired low scale character of the area.

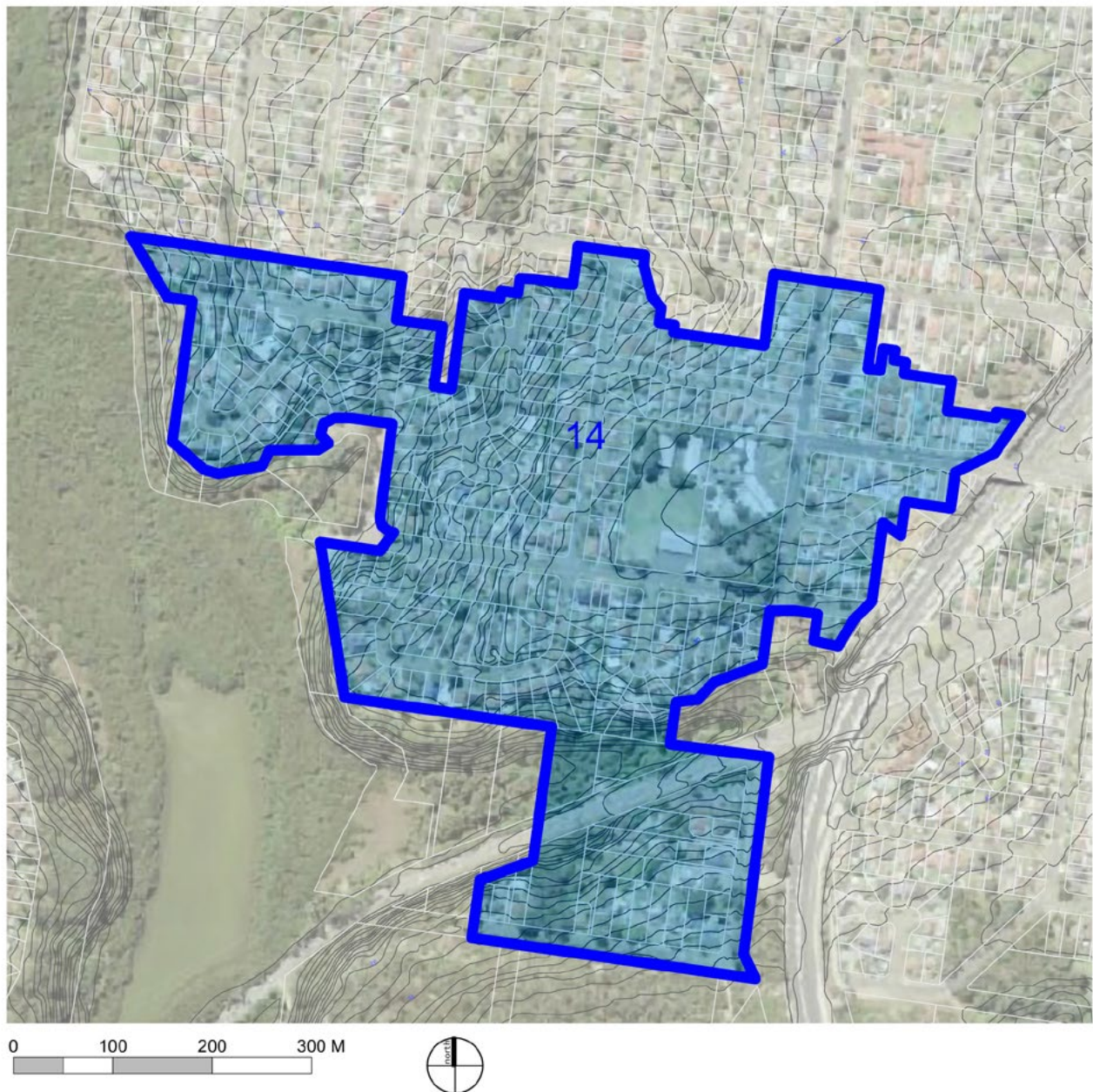
Pedestrian entries should be located forward of the garage, front fencing should be low or open, and front setbacks should feature soft landscaping.

5.13 RECOMMENDED PROVISIONS

OBJECTIVES AND CONTROLS

Objective		Recommended Controls
O1 Enhance the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
	C2	Minimum lot size is to be 450 m ²
	C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
	C4	Minimum front setback is to be 5.5m
	C5	Maintain residential side setbacks consistent with the established street pattern.
	C6	Minimum side setback to be 0.9m clear of obstructions
O2 Enhance the public and private domain interface	C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
	C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
	C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
	C10	Maximum height of front fence to be 1.2m
	C11	Provide street trees of an appropriate native species and increased in numbers.
	C12	Provide footpaths to enhance pedestrian amenity
O3 Maintain the low scale of the LCA.	C13	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O4 Maintain the articulated built form which characterises the area.	C14	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
	C15	Provide a medium pitched primary roof form consistent with the area.
O5 Enhance the range of materials and colours.	C16	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
	C17	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
	C18	Provide materials and colours that are light or mid-tone and complementary with the existing finishes in the area.
O6 Minimise the visual impact of off street parking on the streetscape.	C19	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision
	C20	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
	C21	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

5.14 LOCAL CHARACTER STATEMENT



WEST PADSTOW HEIGHTS

This Local Character Area is located adjacent to the southern boundary of the Canterbury Bankstown LGA. The LCA bound to the west by Little Salt Pan Creek and the boundary with the suburb of Revesby Heights. The eastern boundary is formed by the Davies Road which separates the LCA from the east of the suburb. The northern boundary follows the boundary between the LCA and the suburb of Padstow. The Georges River National Park forms the southern boundary of the LCA.

The LCA is located in an area of changeable topography. The high points in the LCA are a series of ridges located towards the east and north-west of the LCA. From there the lands descends into the valleys located towards the western boundary, and down to Henry Lawson Drive which runs through the south of the site. Many of the streets and residential are located sloping topography, however despite this the road network is generally rectilinear with some variation due to the changing topography. The area is surrounded by natural bushland which can be seen from most locations throughout the LCA. Sweeping regional towards the west and south can be gained from the higher elevations and are a key characteristic of the area.

5.14 EXISTING CHARACTER

The lots within the area generally have a rectilinear form aligned with the street layout. Some lots feature skewed side boundaries of irregular front boundaries can be found on the are curved streets in the area. The predominant residential lot size in the area is between 550 – 600sqm and lot widths are consistently between 15.2-16m wide. There are a number of lots in the LCA featuring larger site areas of 600 – 800sqm. However as the increased lot area is normally due to lot depth, rather than an variation from the prevailing lot width. Residential front setbacks consistently fall between 6.5-8m. While some lots feature larger font setbacks, these are generally limited to the deepest lots or lots located at the end of cul-de-sacs.

The area is a low density residential area and the primary building type is detached residential development on single lots. Residential building heights are generally one or two storeys, however the topography of the area often strongly influences both the building envelope and siting which can often alter the relative height of a building when viewed from the street. On sites with cross falls buildings often have stepped forms or garages set below the ground level and many are greatly elevated above or below the street level.

The primary façade material which characterises the housing in the area is face brick which is used extensively throughout the area, and the houses are often reds, tans, and earthy colours reflecting the colours of era each was built. Another common feature between the majority the housing are the tiled pitched roofs which come in a variety of configurations and forms. While some houses have extensive rendered façades, they are few in number and are generally restricted the newest housing in the area. These developments are most likely to feature elements such as boxed balconies and parapets and monochromatic colour palettes which can sometimes combined to contrast strongly with the built character of the area.

The majority of residences do no feature walls or fencing although where present they are generally low and open when used to demark the font boundary. This allows for the extensive front gardens which characterise the area to enhance the amenity of the street. Many gardens include mature native trees which also contribute the tree coverage of the area. Walling is most likely to feature on sloping sites where they often function as retaining structures in a range of finishes including render, brick and masonry. Sites with more substantial elevation changes often incorporate tiered gardens in the front setbacks with some expressing the natural bedrock of the area.

Most properties feature single driveway crossovers leading to single width garages. Where double width garages are present, they often feature a single width street crossover and are configured to with two single width doors instead of double width doors. Some properties feature open covered parking, however this are generally integrated within the main residential building envelope rather than being a detached structure. Many of the quieter streets are used extensively for parking of private vehicles. As these tend to be narrow and are more likely to feature curved form, vehicle movement is often restricted.

The LCA is located adjacent to the Georges River National Park which is resplendent with bushland which surrounds the western and southern boundaries of the area. These spaces strongly contribute to the natural character and identity of the area, however they lack public facilities and are mainly suitable for activities such as bush walking. Residents are able to access a number of formal reserves to north of the LCA. These are close proximity to the Area and are able to support a wide range of informal outdoor actives and events.

The land use within the area is primarily residential, and due to its small size the area does not feature community facilities such as sports grounds or religious institutions. The Padstow Heights Public School is located within the LCA, however there are no early education facilities in the area. The area does not contain any retail offering, however there are a very small number of home businesses which operate within the LCA. Padstow Local Centre to the north of the LCA provides a wide range of retail and services for the area, however most individuals will opt to travel to this area by vehicle or public transport rather than using active transport.

5.14 EXISTING CHARACTER

The area has a high degree of vehicular connectivity with the Interchange at the end of Clancy Street at the east of the LCA providing direct connection to Davies Road and the Sydney metropolitan road network. There are multiple exit points to the north and east of the area providing access to the suburbs to the north and west of the area. The clear road hierarchy and relatively short street lengths makes navigation through the Area by vehicle easy. There are regular bus services running through the LCA providing residents with services to the Padstow Local Centre, as well as to the greater Sydney areas including Central Paramatta. However access to public transport within the LCA is limited to the single bus stop in the east of the area. This can be difficult to access for residents in the west of the LCA and it is inaccessible for those in the far south of the LCA.

The extent of footpaths in the area are limited to the streets in the east of the LCA in close proximity to the school. Pedestrians are forced to use the grass verges, and while these are generally a reasonable width these can be obstructed by powerline infrastructure, and exposed roots and uneven surfaces can be hazardous in some areas. Streetlighting is relatively extensive throughout the area and there is a good level of street tree coverage across the LCA as a whole. The number and quality of the street trees are higher in the west and the south of the LCA with some areas in the north east of the LCA lacking coverage. Street trees are generally mature mid size native species with dense canopies, with some very tall species located in some areas. Overall the walkability is mixed with often steep slopes and lack of footpaths working against the generally high level of shade protection street lighting and visual quality of the area.



Regional views across the changeable topography is a key characteristic of the LCA which should be maintained. Street frontages are normally low and open and feature areas of lawn and gardens which contribute to open feel of the area



Houses located on the steepest slopes in the LCA generally feature brick or masonry retaining walls. These often support substantial tiered lawns which greatly enhance the pedestrian environment.



Most streets feature detached housing from a range of different eras. Common elements between developments such as scale, roof form and materials reinforce the character of the LCA.

5.14 FUTURE CHARACTER

DESIRED FUTURE CHARACTER

The West Padstow Heights Local character area should maintain its existing character as a high quality low density residential area. The primary development type should be low scale detached housing within a landscaped setting. Future development needs to be compatible with and respectful of steeply sloping topography and extensive areas of tree coverage which characterise and define the area.

Care should be taken when allowing alternate residential buildings to ensure they are of a form and Scale consistent with the existing area. Higher density development should be guided by the controls within Councils LEP and DCP. This will ensure that any future development is compatible with the existing character of the area.

The maximum residential building height should be kept to two storeys. Where developments are on sloping sites, they are to be designed to step down or across the landform to lower bulk and scale. Front setbacks should be generous and consistent with the prevailing street setback. Yards should feature soft landscaping and native tree planting should be encouraged to improve microclimate and extend tree coverage through the area. Front fencing, if present, should be low and open to promote the open nature of the streets. Where retaining walls are used in front setbacks they should be as low as practicable and be tiered. This will avoid creating high walls on front boundaries and ensure the public domain is not enclosed.

To ensure they don't become a dominant urban feature, new driveways should be spaced to retain on-street parking, existing grassed verges and street tree planting. Driveways should be single crossovers to minimise hard surfaces in front setbacks and maximum space soft landscaping in the private domain. Driveways are to be light in colour to reflect heat and minimize the heat island effect. Garages are to be located behind the front building line and allow sufficient clearance from the front boundary to accommodate a parked vehicle fully within the bounds of the property. On sites with steeply sloping landforms or natural constraints where this may not be possible, garaging should still be designed and located to not add undue height to the building form.

New buildings should avoid the use of large unbroken areas of monotonous materials and incorporate feature to reduce apparent bulk and scale such as modulated façades and shade elements. Colours and materials should be selected to minimise the urban heat island effect, and to harmonise with the existing character of the area.

Streets should continue to be well maintained however they will benefit from provision of additional streetlighting. Council has the opportunity to improve the level of walkability by extending the existing footpath network, especially into sloped streets with challenging terrain, and infilling street tree planting on streets lacking coverage.

5.14 FUTURE CHARACTER



New two storey residence with an open front garden and single garage setback from the front façade which ensures the landscape takes primacy within the public domain.



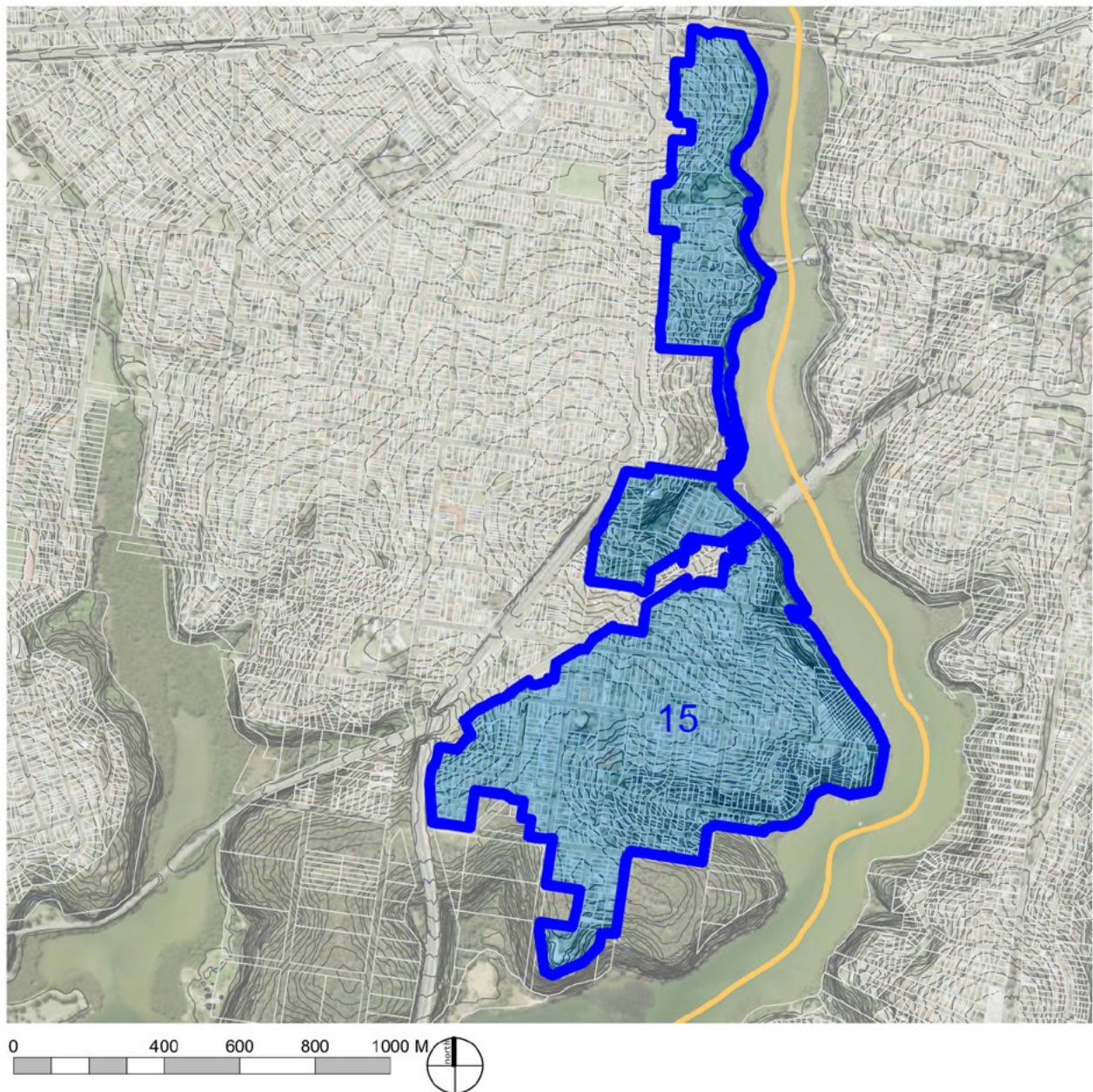
New residences should feature a compact form, modulated façade and medium rise roof. The use different materials and finishes within the building fabric is to be encouraged as it can help to create a fine grain-low bulk built form.

5.15 RECOMMENDED PROVISIONS

RECOMMENDED DEVELOPMENT CONTROLS

Objective		Recommended Controls	
O1	Maintain the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
		C2	Minimum lot size is to be 450 m ²
		C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
		C4	Minimum front setback is to be 5.5m
		C5	Maintain residential side setbacks consistent with the established street pattern.
		C6	Minimum side setback to be 0.9m clear of obstructions
		C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
		C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
		C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
		C10	Maximum height of front fence to be 1.2m
		C11	Provide street trees of an appropriate native species and increased in numbers.
O2	Maintain the low scale of the LCA.	C12	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O3	Maintain the articulated built form which characterises the area.	C13	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
		C14	Provide a medium pitched primary roof form consistent with the area.
O4	Maintain a cohesive palette of materials and colours.	C15	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
		C16	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
		C17	Provide materials and colours that are light or mid-tone and complementry with the existing finishes in the area.
O5	Minimise the visual impact of off street parking on the streetscape.	C18	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision and
		C19	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
		C20	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

5.15 LOCAL CHARACTER STATEMENT



PADSTOW HEIGHTS AND EAST PADSTOW

The Local Character Area is located in the south of the Canterbury Bankstown. The area covers the east of the suburbs of Padstow and Padstow Heights adjacent to the Salt Pan Creek which marks the eastern boundary of the Area. Davies Road forms the western boundary of the LCA and marks the transition between the riverside character of the LCA with the more suburban character of the areas further west. The east and west boundaries converge towards the narrow northern boundary which is formed by the T8 Trainline. The south of LCA is bound by the Nature Reserves and Bushland which are elevated above the Georges River.

There are multiple high and low points throughout the LCA and Most of the LCA is located on changeable topography. This is most apparent in the south and east of the LCA where the difference in elevation between the peaks and valleys is often pronounced and feature a steeply sloped form, where as the slopes in the west and the north of LCA tend to be gradual but fall over a longer distance.

5.15 EXISTING CHARACTER

The north of the LCA features a small interconnected road network whereas a much larger rectilinear road network has been established in the south of the LCA. The longest roads in the LCA often feature a gentle slope with a single rise and fall which run the length of the street. The shorter secondary streets are more likely to feature shorter but steeper slopes which is especially common on the streets terminating in cul-de-sacs. There are multiple opportunities for regional views from the elevated areas within the LCA, with the longer streets and junctions often providing clear sight lines. Views through the area to the extensive bushland which lines the eastern boundary and surrounds the south of the LCA are common throughout the area.

The lots within the area generally have a rectilinear form which is aligned to the street layout with irregular lots only seen on curved roads or on cul-de-sacs. Lot width is most commonly between 15.2-16.5m however there is variance across the LCA with individual or groups of lots featuring small widths of 12.5m, and wider widths of over 17m not uncommon. Lots generally have a size of 550 – 600sqm which applies to a range of lots including narrow and deep lots, as well as wide shallow lots. Larger lots of 600-750 sqm are common throughout the area and are often deep or irregular lots such as those on cul-de-sacs or corners. The largest of over 950sqm lots in the LCA are generally on the deepest lots, however as frontage widths are consistent with the adjacent smaller lots this is often not apparent from the street. Residential setbacks within the area are consistently between 6.5-8.5m with variation to this rare and generally limited to sites with sloping topography where an increased setback is required to allow for vehicle access.

Detached residential dwellings on single lots are the primary residential building type which characterise this low-density residential area. Alternate residential typologies include a small number of multi-residential developments and there are some instances of recently developed dual occupancies. Building heights are normally one or two storey featuring pitched roofs, with limited exceptions on steeply sloping which may feature a taller built form that has adapted to the landform. The topography often influences the siting of individual dwellings with some greatly elevated above, or below the street level. This is most common on the lots located towards the eastern boundary of the LCA and also those in close proximity to Valley Road.

The older housing in the LCA is most often clad in weatherboard or fibre cement and features tiled pitched roofs in a single storey built form. There are generally located adjacent to newer detached residential dwellings of one or two storeys under a tiled pitched roof and are commonly finished in face brick or are fully rendered in some instances. The newest housing in the area, commonly constructed from the 21st century onwards are generally two storeys in height often feature multiple materials such as a combination of render and brick within a façade. Dual occupancies are more likely to feature non-traditional roof forms, extensive parapets and other elements, along with material such as high gloss cladding. The small number of multi-residential developments in the area have a one or two storey built form, are finishes in face brick, an feature front and side setbacks which are consistent with the area. The majority of new residential developments are consistent with the character of the LCA. However some built form outcomes, most often on dual occupancy developments, are incompatible with the character of the LCA due to their bulk, scale and materiality.

Front boundary lines in the area feature a mix low fencing, walling or open boundaries with set in front of areas of lawns. Many properties feature planting forward of the building line, and substantial gardens are common throughout the area which contribute the quality of the public domain on many streets. The most substantial walling in the LCA are on the lots with sloping sites which are raised above the street. These are often retaining walls constructed out of brick or stone masonry, which are incorporated into tiered gardens and lawns.

Offstreet parking in the area is a diverse range of garaging, open carports and uncovered parking. The older houses are most likely to feature and open carport structures with steel construction and flat roofs located to the side of a property or sometimes forward of the building. Garaging in the area is generally incorporated into the main building envelope and is a mix of single garage and two car garages. The larger garages present to the street with two single garage doors located side by side although wider double width doors are becoming more common in newer single dwelling developments. Crossovers

5.15 EXISTING CHARACTER

in the area are generally well spaced single width leading to range of driveway configurations, with double width crossovers limited to sites with wider garages. The use of public streets for parking of private vehicles appears to be most common on the quieter streets which tend to be narrow and may easily become congested, especially in areas with and increased number of higher density residential development types.

The area is characterised by the natural environment with waterways, parks and reserve running the length of the eastern and southern boundaries. Thorpe Park and Beauty Point Reserve on the southern boundary of the LCA feature large expanse of bushland which line some of the residential streets. The Salt Pan Creek Reserve on the eastern boundary features the river walkway which provides a pedestrian link between the north and south of the LCA. Within the LCA are a number of parks of differing character from which are capable of hosting a range of activities. These include Cutting Reserve, Redgum Reserve and Rorie reserve in the north of the LCA, with Villers Reserve and Roma reserve in the South of the LCA.

The area features a small number of businesses and a café which are located in the west of the LCA on the corner of Villiers Road and Dilke Road. However, as the area is primarily residential with the majority of commercial activity limited to home businesses. The Padstow Local Centre offers a large range of retail and services options for those in the LCA. This is a short distance from the north of the LCA makes active transport a viable option, however residents in the south of the LCA are likely to use either public transport or private vehicle to reach the centre. The south of the LCA features aged care facilities the eastern and southern boundary. The is one preschool is located in the north of the LCA however there are no early learning options in south of the area, and the LCA does not feature any schools.

The area has a high degree of vehicular connectivity to the wider Sydney metropolitan road network as there are multiple entry points from the north of the LCA onto Davies Road which runs the length of the western boundary. The south of the LCA has limited entry points onto Henry Lawson Drive which provides access to the adjacent suburbs, but also forms a barrier to vehicle movement between the north and south of the LCA. Travel distances within the north of the LCA are short due to the narrow width of the area. The south of the LCA features a clear road network which is reasonably permeable allowing for good movement. Some streets are interrupted due to abrupt changes in landform, however many of these feature links which allow for continued pedestrian movement whilst preventing vehicle access. The north of the area is well services by a number of bus routes easily accessible on the western boundary. A single bus route operates within the South of the LCA providing a link to the Padstow Local however bus stops are only located on a single road which may be difficult to access from the sometimes challenging pedestrian environment.

The extent of footpaths within the area is mixed with no coverage in the north of the LCA, and footpaths mainly located on key pedestrian routes near to the aged care facilities in the South and East of the LCA. There are reasonably wide grassed curbs throughout the area which pedestrians will need to navigate. This can make walkability challenging in some instances of steep slopes and areas where the lack of permeability results in long travel distances, however this is sometimes mitigated by the pedestrian links which run between roads. Street tree coverage in the area is a reasonable quality with regular coverage on most streets featuring native species with well-developed canopies. Some streets lack planting which is most often seen on the on the longer east-west facing streets. Street lighting can be sparse in some areas especially in the west and south of the LCA, however the majority of the streets have a reasonable level of coverage which will aid pedestrian and vehicle movement.

5.15 EXISTING CHARACTER



The oldest housing in the LCA generally have a small footprint, medium rise roof form and are often finished in masonry brick. Low for open front fencing and they are set back within a landscaped setting which contributes to the open streetscape.

Whilst they predominantly feature single storey height, the building apron can be raised in areas of sloping topography, although they still generally retain their compact form.



Regional views across the changeable topography is a key characteristic of the LCA which should be maintained. Street frontages are normally low and open and feature areas of lawn and gardens which contribute to open feel of the area.



Most streets feature detached housing from a range different eras. Common elements between new and older housing such as roof form, materiality and scale can to reinforce the character of the LCA. However this can be lacking in some instances where there are shifts in scale, setbacks, form and between old and new developments.

5.15 FUTURE CHARACTER

DESIRED FUTURE CHARACTER

The Padstow Heights and East Padstow Local character area should maintain its existing character as a high quality low density residential area. The primary development type should be low scale detached housing within a landscaped setting. Care should be taken when allowing alternate residential buildings to ensure they are of a form and scale consistent with the existing area. Higher density development should be guided by the controls within Councils LEP and DCP. This will ensure that any future development is compatible with the natural setting and existing character of the area.

The maximum residential building height should be kept to 2 storeys. Where developments are on sloping sites, they be designed to step down or across the landform to lower bulk and scale. Front setbacks should be generous and consistent with the prevailing street setback. Yards should feature soft landscaping and native tree planting should be encouraged to improve microclimate and extend tree coverage through the area. Front fencing, if present, should be low and open to promote the open nature of the streets. Where retaining walls are used in front setbacks they should be as low as practicable and be tiered. This will avoid creating high walls on front boundaries and ensure the public domain is not enclosed.

To ensure they don't become a dominant urban feature, new driveways should be spaced to retain on-street parking, existing grassed verges and street tree planting. Driveways should be single crossovers to minimise hard surfaces in front setbacks and maximum space soft landscaping in the private domain. Driveways are to be light in colour to reflect heat and minimize the heat island effect. Garages are to be located behind the front building line and allow sufficient clearance from the front boundary to accommodate a parked vehicle fully within the bounds of the property. On sites with steeply sloping landforms or natural constraints where this may not be possible, garaging should still be designed and located to not add undue height to the building form.

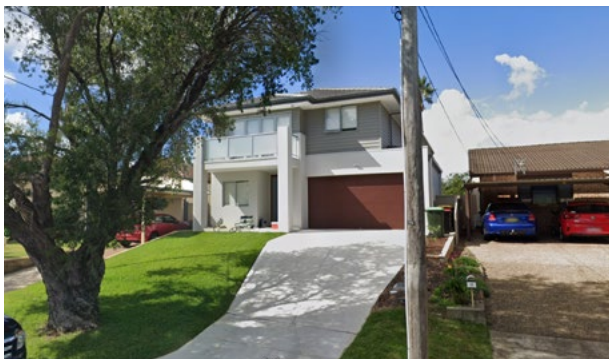
New buildings should not feature high gloss materials, large unbroken areas of monotonous materials, extensive use of parapets. They should incorporate features to reduce apparent bulk and scale such as modulated façades and shade elements. Colours and materials should be selected to harmonise with the natural setting in the area. This will help to ensure that future built forms located in potentially visually sensitive areas such as adjacent to Salt Pan Creek, or on elevated sites located within areas of extensive tree cover, do not become an intrusive element.

Streets should continue to be well maintained however they will benefit from provision of additional streetlighting. Council has the opportunity to improve the quality of the pedestrian environment by extending the existing footpath network, especially into slopes areas of challenging terrain, and infilling street tree planting to increase tree canopy coverage.

5.15 FUTURE CHARACTER



Future residential development should be at a height consistent with desired low scale character of the area. The use different materials and finishes within the building fabric is to be encouraged as it can help to create a fine grain-low bulk built form.



The use of a good mix of materials and colours, deep eaves, a prominent entry, and a first floor setback behind the building will break down the bulk of a built form and create interest in the facade.

The setback garage, narrow driveway, open front boundary, and large areas of soft landscaping which should mature with dense shrub coverage will enhance the quality of the streetscape.

5.15 RECOMMENDED PROVISIONS

OBJECTIVES AND CONTROLS

Objective		Recommended Controls	
O1	Maintain the low density residential character of the area within a landscaped setting	C1	Refer to development controls in the consolidated LEP and DCP for planning framework.
		C2	Minimum lot size is to be 450 m ²
		C3	Setbacks to the building to align with average front setback of houses on two lots on each side of the subject lot.
		C4	Minimum front setback is to be 5.5m
		C5	Maintain residential side setbacks consistent with the established street pattern.
		C6	Minimum side setback to be 0.9m clear of obstructions
		C7	Minimise hard surfaces and maximise soft landscaping in front setbacks
		C8	Minimum deep soil in front setback to be 25% of front setback area and 2.5m minimum dimension
		C9	Maintain front fencing as low and open, at a scale consistent with the LCA. Fences are to be of a material and style that reflects the style of the primary building.
		C10	Maximum height of front fence to be 1.2m
		C11	Provide street trees of an appropriate native species and increased in numbers.
O2	Maintain the low scale of the LCA.	C12	Limit building height to a maximum of two storeys in order to maintain the low scale of the area.
O3	Maintain the articulated built form which characterises the area.	C13	Include modulated façades and incorporate shade elements to break down apparent bulk and give depth to the façade.
		C14	Provide a medium pitched primary roof form consistent with the area.
O4	Maintain a cohesive palette of materials and colours.	C15	Avoid the use of large, unbroken, areas of monotonous surface treatments that are not consistent with the existing character.
		C16	Reduce apparent bulk with the use of different materials and finishes, for example on an entry portico and the main façade, or on different storeys.
		C17	Provide materials and colours that are light or mid-tone and complementarity with the existing finishes in the area.
O5	Minimise the visual impact of off street parking on the streetscape.	C18	Maximise areas for grassed verges and street tree planting by providing driveways that are located to retain on-street parking provision
		C19	Allow garages behind the front building line to ensure they do not become the predominant façade element when viewed from the street
		C20	Locate sufficient space between the front boundary and the garage to accommodate a parked vehicle fully within the bounds of the property.

OLSSON

ARCHITECTURE | URBAN PROJECTS